

Felton Town Plan

*A Specific Plan for
the unincorporated
Village of Felton*

Prepared for:
**Santa Cruz County
Board of Supervisors**

By:
**Jeff Oberdorfer & Associates
Architecture/Community Planning**

Felton Town Plan



The Russell & Ley buildings with their western false front facades characterized Felton around the turn of the century. These buildings were located at the crossroads of what is now the Highway 9/Graham Hill Road intersection. Photo Courtesy: Santa Cruz County Historical Museum.

Adopted: July 28, 1987

Board of Supervisors Resolution—No. 521—87

Joe Cucchiara
Dan Forbus
Robley Levy
Sherry Mehl
Gary Patton

Approved: May 27, 1987

Planning Commission Resolution—No. 18—87

Winifred Alexander
Myrna Britton
Ree Burnap
Denise Holbert
Frank Osmer

Planning Department Staff Liaisons

Dave Laughlin
John Warren
Steve Matarazzo

Prepared by: *Jeff Oberdorfer & Associates*
Architects/Community Planners
303 Potrero Street
Suite 29-306
Santa Cruz, CA 95060

Design Team: *Jeff Oberdorfer, Keri Somers, James Young, Betsy Alden*

Pedestrian Mapping Team: *Jessica M. Paul, Naoko Kathryn Miura, Eric Novak, Will Baylis*

Pedestrian Map Graphics: *Jessica M. Paul, Jeff Oberdorfer*

Transportation Planning: *DKS Associates*

Construction Cost Estimator: *Donald W. Elder*

Workshop #5 Panelists: *Lee A. Strong; Principal, Downtown Focus and Steve Costa; President, Bay Area Partners*

Contents

Introduction	1
Community Participation Process	2-8
• Diagram of Participation Process	2
• List of Workshop Participants	3
• Workshop Process Description	4-8
Existing Felton Village Map	9
Pedestrian Mapping Plans	10
Community Commercial District Plan	11
Utility District Map	12
Graham Hill Road Entryway	12
Scenic View Corridors Guidelines	13-17
• Scenic View Corridor Map	13
• Scenic View Corridor #1	14
• Scenic View Corridor #2	15
• Scenic View Corridor #3	16
• Scenic View Corridor #4	17
Historic Resources	18
Felton Covered Bridge	19
Felton Commons	20
Flood Plain	21
Open Space and Recreation	22
Circulation and Parking	23-26
• Circulation Improvements Map	23
• Circulation and Parking	24-26
• Equestrian Circulation	26
Village Design Opportunities	27-34
• Village Design Concept Plan	27
• Village Entry Sketches	28
• Highway 9 Concept Plans	29-30
• Pedestrian Focal Points	31-32
• Infill and Parking Opportunities	33-34
Townscape Design Guidelines	35-37
Townscape Facade Guidelines	38-42
• Eastside Highway 9	38-40
• Westside Highway 9	41-42
Start-up Projects	43
Cost Estimates	44

Recommendations

Throughout the Felton Town Plan booklet, proposed Policies and Recommendations are listed in boldface format.

Community Commercial District Policy	11
Utility District Recommendation	12
Historic Resources Policy	18
Felton Covered Bridge Policies	19
Felton Commons Policy	20
Open Space/Recreation Policies	22
Bicycle Circulation Recommendations	25
Summary of Circulation/Parking Recommendations	25
Highway 9 Off-street Cooperative Parking	29
Policy for consistent setback lines on Highway 9 in the Community Commercial District	35
Heritage Tree Policy	37
Library/Post Office Policies	37

Introduction

Purpose

The Felton Town Plan has been prepared to establish design and development guidelines for the village center of Felton. The plan will be used to guide and coordinate future public and private developments in this town center.

The Felton Town Plan is prepared in accordance with the County General Plan policy calling for the creation of village design plans for the community centers in the unincorporated portions of the County. The Felton Plan is the second village plan to be prepared in a series of nine village plans identified in the General Plan. The village plans are based on a public planning process as outlined in General Plan program 8.4.101:

- Prepare and adopt Village Design Plans, with input from citizen's task forces, for the village areas in Soquel, Live Oak, Boulder Creek, Ben Lomond, Felton, Davenport, Freedom and La Selva Beach. Include in the Village Design Plans a community design framework, priorities for capital improvements, traffic and parking plans, sign plans, and specific provisions for commercial and residential land uses and siting. Consider Village Design Plans as Specific Plans and as Amendments to the County General Plan where the plans conflict with any portion of the County General Plan.

The Felton Town Plan is intended to provide a more detailed examination of this community center of social and commercial activity than could be included in the County-wide General Plan. The goal of the village plans is to preserve and build on the unique attributes of each of the village areas as stated in General Plan Objectives 8.4.1 and 8.4.2:

- To preserve the unique characteristics of the village areas as community focal points for living, working, shopping and visiting.
- To provide a planning framework to guide future public and private improvements in the village areas and to promote economic viability and coherent community design.

The Felton Town Plan will be used for a variety of purposes to enhance the village center. The Plan identifies community needs for services and improvements, and will be used to develop public and private programs to address those needs. The plan provides suggestions for the use and improvement of private property, and is intended to stimulate private implementation of improvements to the village. The Felton Town Plan establishes development standards to be applied to new development through the

design and review of new projects as specified in General Plan Policy 8.4.3:

- Review all new development in Village Areas, as defined in adopted Village Design Plans, through the Planned Development Permit process and require compliance with all requirements of an adopted Village Design Plan for a Village Area. Encourage all landowners and businesses in the Village Areas to follow the guidelines adopted in the Village Design Plans.

Study Area

Felton is an unincorporated village located in Santa Cruz County approximately 80 miles south of San Francisco and 20 miles south of the San Jose metropolitan area. The city of Santa Cruz is approximately 6 miles south of Felton.

Felton is the gateway village to the San Lorenzo Valley, an area of steep, redwood hills and a narrow valley bottom along the San Lorenzo River. The San Lorenzo River flows through Felton just east of the Village and downstream to the city of Santa Cruz where it enters Monterey Bay and the Pacific Ocean.

Visitors travel from all over the world to visit the historic Felton Covered Bridge (built in 1892) and to ride historic steam trains through the redwoods at Roaring Camp Big Trees Railroad.

In order to analyze area wide circulation, open space needs, scenic views and historic resources, a broad view of the Felton area was undertaken. However, specific design and planning analysis was focused upon the Felton Village Core.

The Planning Process

The Felton Town Plan was developed in partnership with local citizens who contributed their ideas and visions during a community participation process which involved over 140 local people in a series of five Town Plan Workshops.

An analysis of participants' favorite places and favorite experiences in Felton was developed and coordinated with observational analysis of pedestrian circulation. This analysis revealed what people in Felton thought special about their town and how they used the existing "downtown" area. This data was collected by the consultant team and became an integral part of the citizen workshop process.

General Study Area

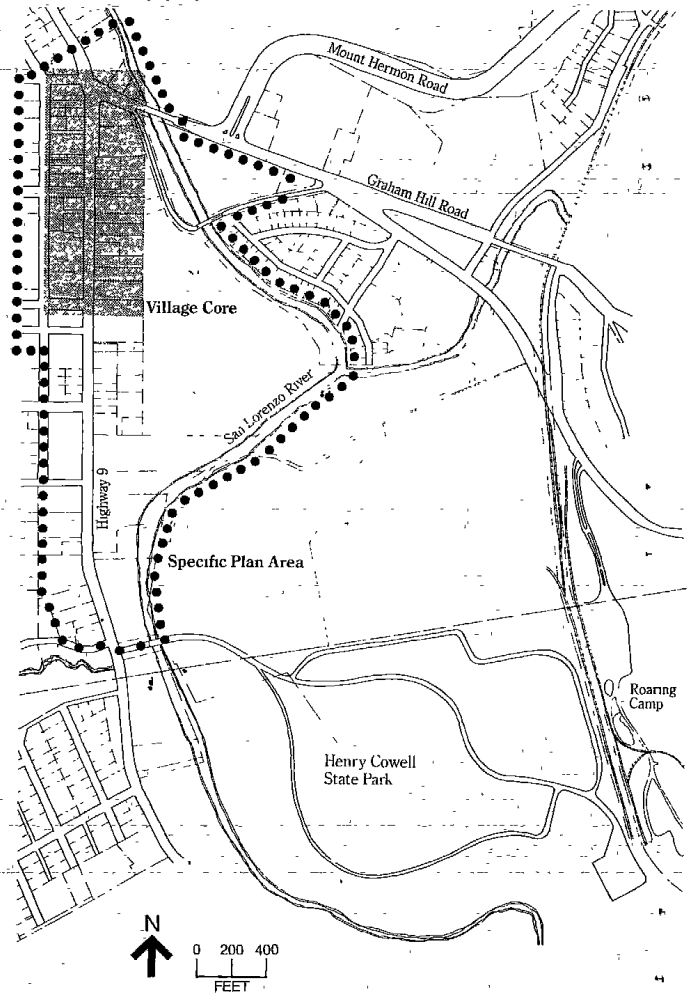
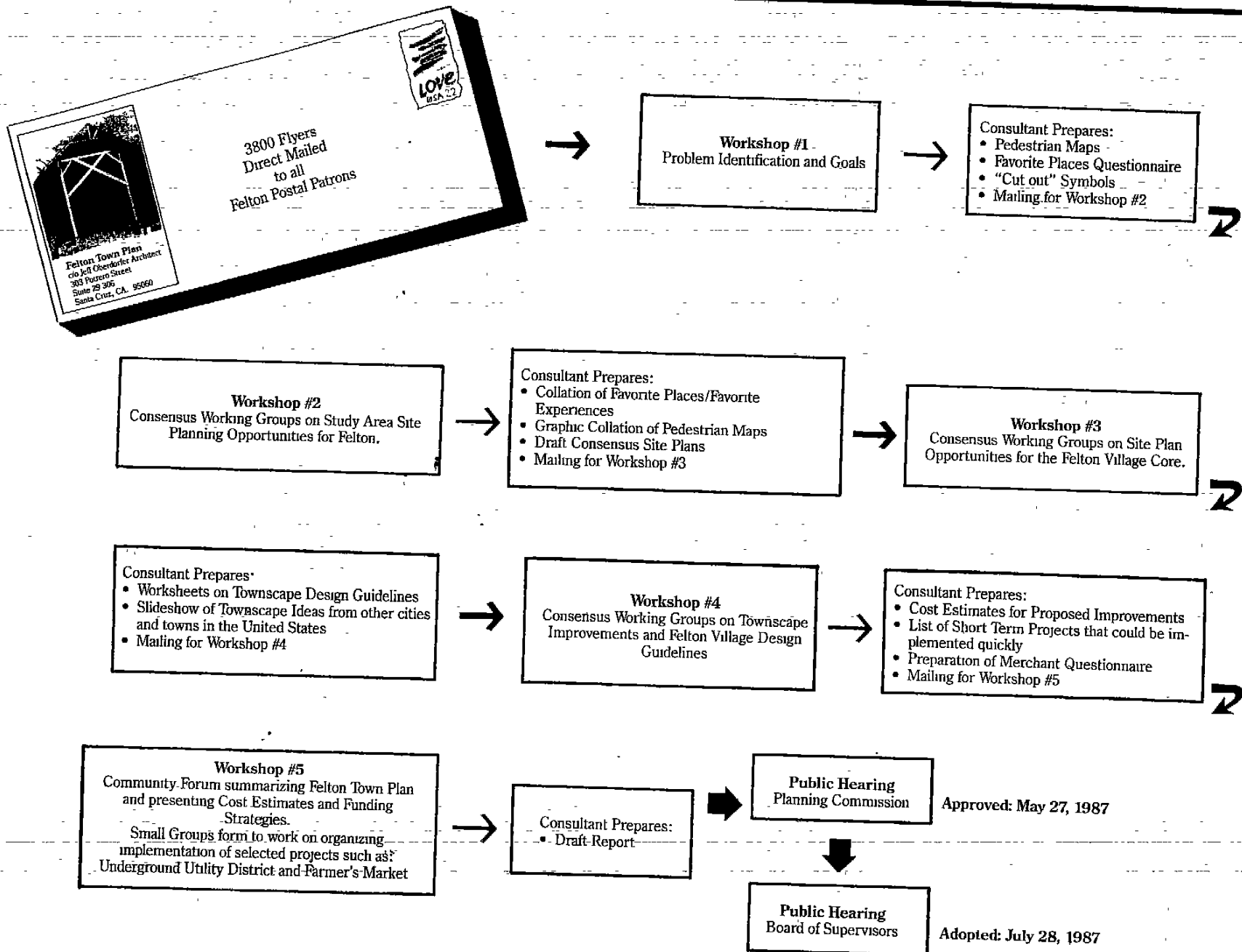


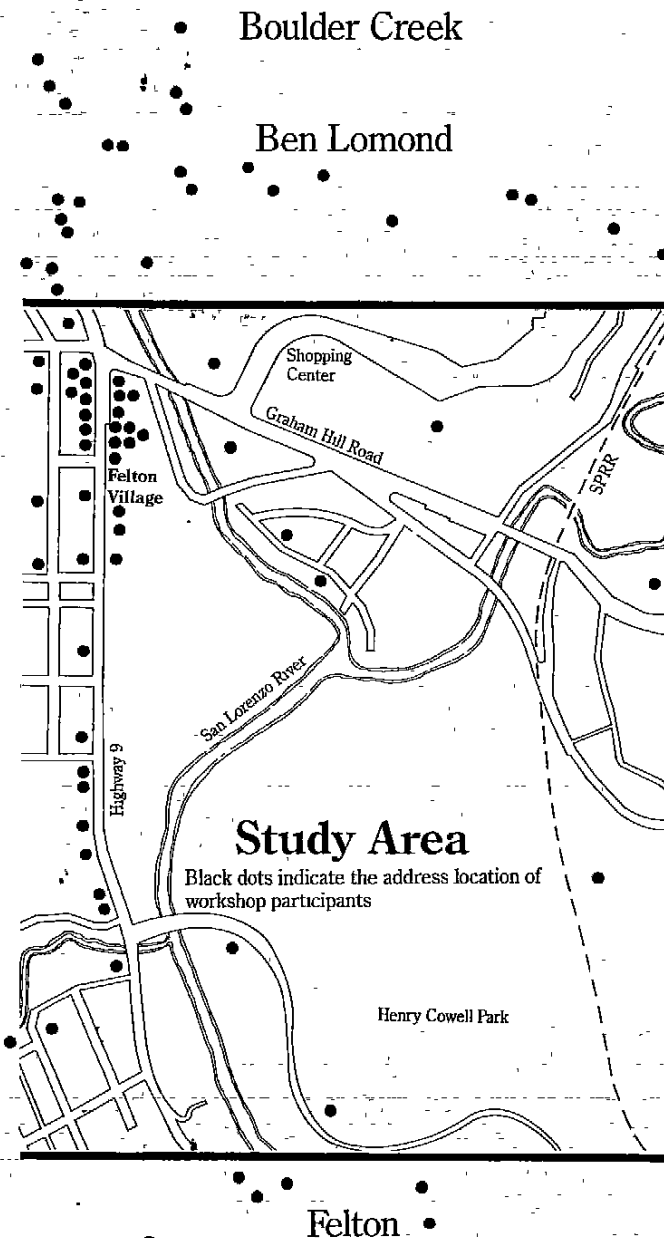
Diagram of Participation Process



Workshop Participants

Gene Carl Koch
 Russ and Ellen Aucoin-Urruhe
 Sally Moller
 Bruce A. Moller
 Ken Burke
 Kate K. Stafford
 Dave and Roberta Gutierrez
 Donald G. Ley
 Cheryl Anderson
 Virlyn Gouin
 Judy Stoddard
 John K. Masten
 Barbara and Robert Lewis
 Gene and Beverly Martin
 Sally and Fred Woolley
 Susan Elgin
 Julie Erreca
 Pat Verruti
 Renee and Michael Shepherd
 Clifford Shurt
 Stephen Witse
 Roy's Market
 Bob Locattelli
 David Schmedes Plumbing
 Richard Palmer
 Alan Jurgens
 Mike Burns
 Ron Sekkel
 Steve and Beth Egelhofer
 Wayne and Katie Hendrickson
 Rod Jensen
 Ed and Gail Hill
 Kelly Wood
 Mr. and Mrs. Paul Schmidt
 Ken Clouse
 Nancy Hendee
 Oliver L. Wright
 Joyce Sutherland
 Neil McGee
 CA. Dept of Forestry
 Tom Peters
 Jackie Peters
 Jim and Dawn McCarrick
 Mike and Maureen Wirtz
 Vince Waskell
 Dennis Booth
 Jon Kint
 Jim Bowdoin
 Ed and Marge Viera
 Jim and Helen Gothrop
 Jack Schneider
 Tanya Niles
 Lee Haskin
 Richard Larsen
 Joe Patzke
 Steven Leib
 Bill and Carol Bray
 Richard Reed

Mr. and Mrs. Lester Howe
 John Prieskorn
 Carol Raynsforth
 Frances Adamson
 Michael Rugg
 Roy Gomez
 James Rhoades
 Robin Musitelli
 Linda Stone Beach
 Don A. Wood
 Rene Leaman
 Jeanne Thomas
 Lawrence Zimmerman
 Cathy Calfo
 Gerald Fialho
 Joe Cucchiara
 Mike DiDonato
 Mill and Sue Garrett
 Georgie Purtscher
 Tom Fox
 Gary Bullard
 Frank L. Winkler
 Mike Dayton
 Al Johnson
 Martin Prehm
 Joan Surber
 Margaret Ingraham
 Carrie Kojak
 Isabel Kelly
 Arthur A. Bubb
 Hervey and Opal Rainville
 Lawrence and Dawne Harman
 Cliff and Norma Short
 Tim Clouse
 Frank Winkler
 Greg and Carmel Crowther
 Barbara Kiehl
 Karen Bailey
 Carole Drexler
 Nora Jansen
 Marilyn Robertson
 David Bernick
 Janet Fabula
 John Mott
 Robert George
 Faye Ellis
 Peter and Ginny Katzberger
 Paul Eller
 E.H. Gransbury
 James Luke
 Steve Homan
 Anthony and Holly Tyler
 Shirley J. Adams
 Sue Garrett
 Todd Prindle
 Kent Washburn
 Neil McGee
 Terry Dovey



Workshop Process

The visions and concerns of Felton residents, business owners, merchants and other interested citizens were an integral part of a planned participation process. This process formed the core of the decision making component of the Felton Village Master Plan.

Participants worked together in small groups. Each group developing plans for physical improvements and revitalization efforts for Felton. The proposals of each group had to be agreed upon by everyone in the group. At the end of each workshop, working groups compared their consensus plans with the plans of from four to eight other working groups—noting the similarities and differences amongst the various plans.

Summaries of each workshop were mailed to all participants and reported in the local newspapers. Each workshop built upon the decisions and ideas generated in the preceding workshop.

Workshop #1

This workshop was a brainstorming session whose focus was to discuss physical problems that currently existed in Felton and to establish a wish list of goals for Felton's future. 95 people attended this workshop. Problems and goals were graphically recorded on large sheets of paper in the front of the room and where possible were illustrated on large maps of the Felton Area. There were no attempts to prioritize the problems or goals listed by workshop participants. The following is a summary of Workshop #1:

Problem Identification:

Traffic and Circulation

- Congestion at Highway 9/Graham Hill Road intersection.
- Congestion at Graham Hill Road/Mount Hermon Road.
- Intersection safety concerns at Graham Hill Road/Zayante Road and the Graham Hill Road/Roaring Camp entryway.
- Speeding southbound on Mount Hermon Road.
- Speeding on San Lorenzo Avenue, eastbound to Highway 9.
- Vehicular and pedestrian safety on Highway 9, north of Graham Hill Road and near the High School.
- Turn from Covered Bridge Road North onto Graham Hill Road is dangerous.

- Right turn north from Graham Hill Road to Highway 9 is dangerous.
- Pollution from vehicles at congested intersections.

Parking

- Inadequate parking and dangerous backing out movements from on-street perpendicular parking on Highway 9.
- Need for share-ride commute parking lots.
- Parking needed near Library on Gushee Street.

Lighting

- Need lighting at Highway 9/Graham Hill Road Intersection.
- Need lighting for pedestrian safety and to decrease vandalism at both ends of the Felton Covered Bridge.

Signage

- Existing signage is unsightly and lacks any design guidelines.
- Signage and route designation to Loch Lomond Reservoir are poor.

Drainage

- Drainage is an area-wide problem, specifically on Highway 9, south of Graham Hill Road.
- Maintain drainage gorge near Mill Creek at Redwood Drive.
- Define who is responsible for area-wide drainage planning and maintenance.


Goals:

Infrastructure

- Develop strategies and organization to maintain and finance proposed improvements.
- Provide area-wide waste and septic strategies.
- Underground utilities on Highway 9 from Hihn Street to Graham Hill Road.
- We need better telephone service.
- Sidewalks needed downtown, that provide handicap access and retain Village character.
- "What about the Ben Lomond and Mount Hermon Road bypass proposals?"

Facilities and Uses

- Maintain and preserve existing Library, providing better parking, pedestrian access and outdoor open spaces.
- No fast food places.
- Provide Public Restroom(s).
- Hotel/Motel is (are) needed.
- Develop a Community Garden.
- Encourage residential uses and Felton businesses to stay in the Village.



FELTON TOWN PLAN

Five "hands on" community workshops for local people to develop a Felton Town Master Plan are scheduled. Your input is important!

Workshop #1: Problem Identification & Goals for the Future is scheduled for:

Thursday
February 21, 1985
7:15 - 10:00 PM
San Lorenzo Valley High School
Performing Arts Room

The Felton Master Plan and Workshop Process will be facilitated by Jeff Oberdorfer, Architect. ODA 3DA designed the community participation process for the Boulder Creek Branch Library in mountain wilderness for the Felton Covered Bridge and has recently completed a masterplan for a "magical" Parking Structure for the North Pacific Garden Mall.

3800 Workshop flyers were direct mailed to all Felton Postal Patrons and Post Office Boxes.

Horses

- Provide horse trails through parks and from Graham Hill Road to Henry Cowell Park entry on Highway 9, and on road shoulders on Highway 9 and Graham Hill Road.
- Provide convenient stalls to rest horses.
- Preserve existing stables.
- Provide roadside warning signs at equestrian crossings.
- Horses and horse-back riding are an important part of Felton's character, that should be retained.

San Lorenzo River

- Provide views to the river from the downtown village area.
- Consider developing restaurants or other businesses with views and/or decks overlooking the river.
- Develop pedestrian trails to the San Lorenzo River.
- Develop Riverfront park.
- Develop swimming holes and boating (possibly adjacent to Santa Cruz Water Department property near the Henry Cowell entryway on Highway 9).

View Sheds/Vistas

- Maintain and enhance views from Mount Hermon Road and Graham Hill Road westward into the Felton Village.

Tourism

- Bring tourists into town.
- Minimize tourists in town.
- Coordinate Roaring Camp, tourist and Park visitor information.
- Develop a trolley loop from Roaring Camp to Mount Hermon Road to the Village and back.

Traffic/Parking

- Slow down traffic on Highway 9.
- Provide short-term parking.
- Separate vehicular traffic from pedestrian, bicycle and equestrian circulation.
- Provide revenue by renting bicycles and battery powered vehicles.
- Develop park and ride locations.
- Provide turning lanes where appropriate to reduce congestion.
- Provide additional parking near Tampico Grande.
- Take cars out of the downtown area.

Parks and Open Space

- Develop a park on the property north of the Covered Bridge and incorporate a tot-lot, play-area and parking.
- Provide a tot-lot and childrens play area.
- Provide a swimming pool.
- Develop area north of Henry Cowell State Park and South of Felton Covered Bridge (which is in the floodplain and therefore undevelopable) as a park, and incorporate boating, horsetrails and pedestrian amenities.
- Develop a Par Fitness Course.

Signage

- Develop signage standards.
- Develop entryway signage.
- Develop historic displays and signage.
- Develop downtown visitor maps/displays.

Design Guidelines and Land Use

- Develop Design Guidelines for:
 - a) Signage.
 - b) Building Design.
 - c) Use Permit Guidelines.
 - d) Village Landscape and Planting Plan.
- Provide more benches and pedestrian amenities downtown.
- Locate public art opportunity sites for murals and sculpture.
- Limit allowed uses, not building design freedom.
- Maintain the Village as the primary shopping area.
- No boutiques; Felton isn't the Pacific Garden Mall, Capitola or Carmel.

WORKSHOP PROCESS (Cont'd)

Lighting

- Retain existing light fixtures on Graham Hill Road Bridge.
- Design a night, downlighting system which provides security but maintains a rural character, so that we "can still see the stars at night".

Entryways(s)

- Provide an entryway at Highway 9 and Graham Hill Road.
- Provide an entryway at Graham Hill Road and Mount Hermon Road.
- Coordinate entryway design and location with good year-round and seasonal lighting system.

Describe Felton's Character and Uniqueness:

Town "Memory"

Slow, Organic Change

"Home Made" Products

Historic roots

Multi-Generational Community

Small

Rural

Village

Western

Redwood

Rustic

Natural Beauty and Views

Folksy

Riverfront

Individualistic Architecture

Minimal Change

"Gateway to the Valley"

Vineyards

Not Scotts Valley

Not Capitola

Not Carmel

- Maintain Felton's character and charm.
- Leave Felton just the way it is, with minor improvements.

Workshop #2

In order to further define what workshop participants considered "special" about Felton, a questionnaire was included in the Workshop #2 notification mailing asking for a list of Favorite Places and a list of 5 experiences which contribute to Felton's Special Character. Participants were asked to bring the completed questionnaires to Workshop #2.

The focus of Workshop #2 was overall Site Planning for the Felton Study Area, including; circulation, parking, infill building opportunities, pedestrian amenities and open space planning. 48 people attended this workshop.

Based upon the wish list of goals generated during the first workshop, an 8 1/2 x 11" sheet of symbols representing specific improvements (such as restaurants, restrooms, etc.) and more general site planning elements (such as view corridors and village entryways) was prepared.

Eight groups of 5-8 persons each, spent almost two hours developing consensus site plans. By cutting out and positioning the symbols each group thought appropriate, and then adding additional information with colored markers, a consensus plan was developed by each group.

At the end of the evening, each group presented their consensus site plan and compared their site plan to the others being presented. There was no attempt to prioritize the eight group plans.

Discussion

After the second workshop, the consultants prepared a summary site plan which illustrated the areas of agreement on each of the group consensus site plans from Workshop #2. These areas of agreement included:

- Preservation of views of Felton Village as seen from Mount Hermon Road and from Graham Hill Road entering Felton.
- Preservation of meadows north of Graham Hill Road now used as horse pasture.
- Development of public open space in the triangular parcel north of the Felton Covered Bridge and south of Graham Hill Road.
- Undergrounding of utilities on Highway 9, from Hihn Street to Graham Hill Road.
- Preservation of existing horse stables south of the Felton Covered Bridge.
- Development of infill building sites on the east side of Highway 9, overlooking the San Lorenzo River and the Felton Covered Bridge.

Workshop #2 SITE PLANNING

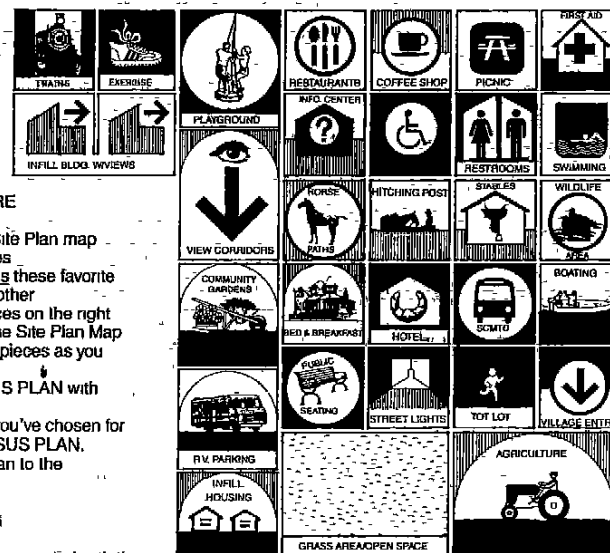
Felton Town Plan

WORKSHOP PROCEDURE

- 1 Place orange dots on Site Plan map indicating favorite places
- 2 Discuss the connections these favorite places have with each other
- 3 As a group, cut out pieces on the night and arrange them on the Site Plan Map. Use as many or as few pieces as you need
- 4 Develop a CONSENSUS PLAN with those at your table.
- 5 Glue down the pieces you've chosen for your group's CONSENSUS PLAN.
- 6 Present your group's plan to the Workshop group.

CONSENSUS PLANNING

Each group plan must be created with the participation and agreement of everyone at your table. You will most likely have to make trade-offs and compromises.



Sample Workshop #2 cut-out sheet, with symbols generated from Workshop #1, Problem Identification and Goals Session. Symbols were cut out then then placed on large maps of Felton.

- Location of pedestrian and equestrian circulation opportunities.
- Use of the west side of Gushee Street for bicycle and equestrian paths.

A summary site plan illustrating these areas of agreement was mailed to each workshop participant with a summary of the Favorite Places/Favorite Experiences questionnaire and notification of the time and place of Workshop #3.



Workshop #2 consensus group cutting and pasting symbols shown at top of page.

Favorite Places

(Collated from 50 returned questionnaires with preference scoring listed next to each Favorite Place)

• Henry Cowell Redwoods	33
• Felton Covered Bridge	31
• Felton Branch Library	29
• Fall Creek/Park	16
• Roaring Camp	15
• San Lorenzo River	13
• Felton Empire Vineyard	9
• Horse Pasture north of Graham Hill Road	8
• Heavenly Cafe	8
• Post Office	7
• Felton Community Hall	6
• Old Cemetery	6
• White Raven Espresso	6
• Patio and Book Store	6
• My house and garden	4
• Roy's Market	4
• Chapparal Corral	4
• Ice Cream Shop	4

Favorite Experiences

Participants were asked to:
 "List 5 experiences you have in Felton on a regular basis which contribute to your impression of Felton's special character and which you don't want to see changed."

"Responses were collated in three categories. Experiences which were of a descriptive nature, experiences that were activity oriented and experiences which were place oriented. The top vote getters are shown in bold type."

Descriptive Experiences

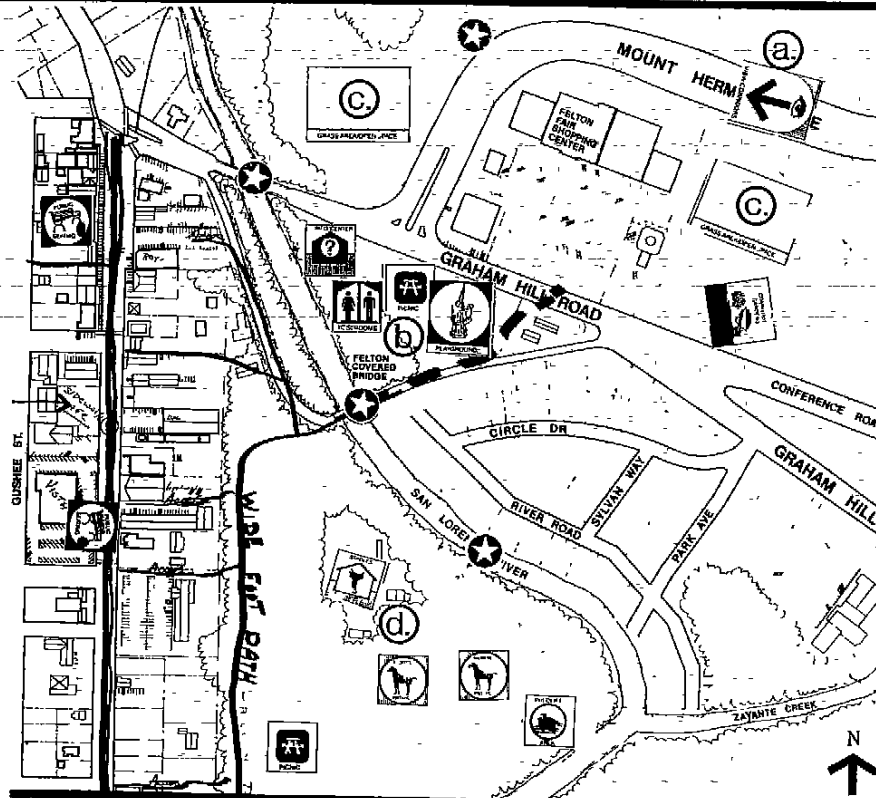
- **Small town atmosphere and charm, unhurried pace, seeing friends downtown, seeing familiar faces, friendliness of people, going to a market and knowing everyone by name, and doctors that make housecalls.**
- **The view of Felton from Mount Hermon Road as you're entering town.**
- Small, local, individually owned shops that aren't crowded and that cater to local needs.
- Hodge-podge planning and architectural style of "old-fashioned" buildings.
- Quietness at night.
- Fresh, clean air.
- The redwoods, beautiful vegetation, wildlife and birds.

Activity-oriented Experiences

- **Walking, running, jogging, bike-riding in Henry Cowell Park.**
- **Horseback riding, horses in town, walking, riding horses along Zayante Road to the Covered Bridge and to Roaring Camp, seeing kids ride horses through town and around the Covered Bridge.**
- Walking around town and in parks.
- Going across the Covered Bridge.
- Swimming, walking, playing along the San Lorenzo River.
- Travelling at dusk across the lighted concrete bridge.
- The ride on Highway 9, through the redwoods to Santa Cruz.

Place-oriented Experiences

- **Watching steam trains at Roaring Camp, hearing the train whistle, feeding ducks and picnics at Roaring Camp.**
- **Picking up mail at friendly Post Office.**
- **Library visits.**



Consensus Site Plan generated by Workshop #2 participants clearly illustrates a) Mount Hermon Road view shed, b) Park/Open-Space activities adjacent to the Felton Covered Bridge, c) preservation of open space east and west of Felton Fair Shopping Center, and d) maintaining existing stables. Stars indicate favorite places

- Fire Department pancake breakfasts.
- Coffee at Heavenly Cafe.
- Availability of Community hall.
- Felton/Gushee Street/Community Hall potlucks.
- Homeness of Felton Bowl.
- Grabbing a sandwich at Roy's Market.



Workshop #3

The third workshop began with a slide presentation of the summary site plan map developed by the consultant and with a discussion of various traffic and circulation schemes that would improve circulation on Highway 9. Making the village core more attractive and safe for pedestrians was discussed as well. 42 people attended this workshop

Participants worked in small groups and focussed upon parking, circulation, land use and the visual appearance of the village core. The village core was defined as the Highway 9 area from Hahn Street to Graham Hill Road, extending east to the San Lorenzo River and west to Gushee Street.

Consensus was developed in each small, working group and then discussed by the group as a whole. Several themes became clear from each of the working group presentations.

- A one-way loop, vehicular circulation scheme using Gushee Street and Highway 9 was unanimously rejected.
- A circulation scheme for angled parking rather than the existing perpendicular parking arrangement on Highway 9 with a landscaped median separating north and south vehicular traffic was accepted, with the following modifications:
 - a. participants requested mid-block U-turns on Highway 9 across from Roy's Market and across from Vista Foods, to provide vehicle access to both sides of Highway 9.
 - b. participants stressed the need for wider, handicapped accessible sidewalks on Highway 9, and suggested narrow, eleven foot wide medians with wider sidewalks.
 - c. participants eliminated trees from the landscaped medians in favor of low, flowering landscaping both to provide better visual access to village storefronts and to maintain the wide open, "western" feeling to the village.
- Gushee Street should be rezoned from commercial to residential on the west side with the east side remaining commercial:
 - a. landscape "buffer" guidelines should be developed for the east side of Gushee Street.
 - b. additional accepted uses on the west side of Gushee Street would be Professional Office and Bed and Breakfast, with a preference for Bed and Breakfast.
 - c. undergrounding of utilities should be considered on Gushee Street.
 - d. bicycle lanes and equestrian paths would be preferable on the west side of Gushee Street rather than on Highway 9; especially for connections to Fall Creek Park
 - e. the west side of Gushee Street should be striped and signed for "no parking" or for "residential parking only."
- There was unanimous agreement that visitor parking for the Felton Covered Bridge should occur at the proposed Felton Commons or in the Felton Village area and not in front of the west entry to the Covered Bridge off Covered Bridge Road North.
- There was general agreement that the access road for the existing horse stables be relocated south of the stables to eliminate vehicle traffic at the west end of the Felton Covered Bridge

Workshop Process (Cont'd)

Workshop #4

The focus of Workshop #4 was for participants to develop guidelines for the following "townscape" elements of the Felton Town Plan:

- Library/Post Office expansion
- Signage/lettering styles
- Facade design on Highway 9
- Building materials
- Color and visual appearance
- Sidewalk materials
- Awnings/canopies
- Lighting

Participants worked in small groups and developed consensus decisions working with hand out sheets provided by the consultants. 42 people attended this workshop

Library/Post Office

Both the Felton Branch Library and Felton Branch Post Office are "favorite places" in Felton. The Post Office for its friendly service and personal scale, and the Library for its scale, responsiveness to local needs and location in the Faye Bilardi Memorial Building (listed in the National Register of Historic Places). Both facilities promote the use of the Felton Village Core.

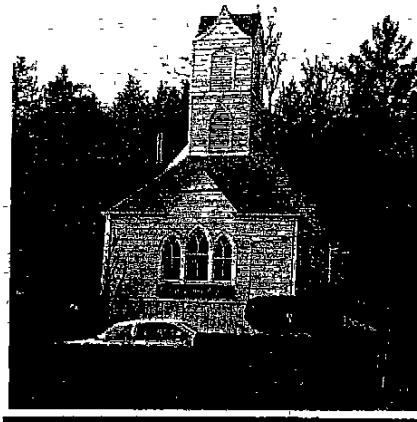
Both the Post Office and Library lease space at locations on the west side of Gushee Street, and both have outgrown their current facilities. While the local postmaster would like to provide new and larger facilities, there are no immediate plans to do so.

The 5 Year County Library Master Plan, however, does propose an expansion of the Felton Library facilities.

The Faye Bilardi Memorial Building, which houses the Felton Branch Library, is leased to community-serving groups as a condition of a Trust established for the building's use. If the building is *not* utilized in this manner, it reverts back to the building's owners.

The Bilardi Memorial Building is listed on the Santa Cruz County Historic Resources Element of the County General Plan. Thus, review and approval is required from the County Historic Resources Commission prior to any remodeling which would impact its visual character.

Given the particular importance of the Branch Library and the historical nature of the building, and given the fact that the Felton area, based upon population statistics, could support a regional library facility, Workshop participants were asked to prioritize the following potential policies:



Faye Bilardi Memorial Building on Gushee Street, used as Felton Branch Library

5. When the Branch Library moves to a new location (if and when this occurs) and no other community-serving uses or groups take over the building's lease, allow the building to be utilized as a Professional Office Building (or other appropriate use) with strict design controls on renovation in order to preserve the character and appearance of the building exterior.
2. Do whatever is required to maintain this historic building as a community-serving use, so that local residents still have access to the building.
3. The community-serving use of the Faye Bilardi Memorial Library is as important to the Felton Community as the building's historic character and appearance
1. Maintain as special collection of the Library System and as Visitor History/Information Center, staffed by volunteers and open with limited hours of use
4. Maintain as Felton Branch Library with improved computer access to the Main Library System, with Felton patrons going to Santa Cruz or Boulder Creek for special needs.

A summary of participants prioritization of each policy is shown next to each of the proposed policies. It is clear that the use and accessibility of the Faye Bilardi Memorial Building is just as important to the people of Felton as its historic appearance, and that some library oriented function such as a special collections and/or information center use is appropriate.

The Felton branch of the U.S. Post Office is located on Gushee Street at the intersection with Huhn Street, and is a **favorite place** in Felton. The post office is currently undersized, however there are no immediate plans to expand the facility or to move to a new location.

Given that the Post Office and Branch Library both will eventually need to expand their facilities, and given their popularity in Felton, workshop participants were asked to rate a series of hypothetical policies which explored creating a joint Library/Post Office in the event of future facility expansion

The combination of the Regional Library and enlarged Post Office Facility is a good idea, as long as both facilities retain their individual scale and identity, but share common facilities such as parking, rest rooms, meeting rooms and open space.

Given the popularity and heavy use of these facilities, these buildings should be combined in a non-institutional manner with emphasis upon their social and community-serving functions. Outdoor seating, play areas, coffee shops and sandwich shops should be developed within a cluster of small-scaled buildings

The combination of these two facilities is not a good idea. Their new, larger size will be too institutional and alter the scale and feeling they now have

The combination of the Library and Post Office is a good idea as a general policy, but there is not enough information now to make an educated decision.

None of these policies were embraced by Workshop participants. Comments included:

a) maintain both facilities as they are, b) shared parking only would be OK, c) separate buildings only, and d) combined facilities only "if done just right".

Design Guidelines

Workshop participants stressed the need for guidelines that allowed property owners and merchants maximum freedom in design, so as to not dictate style, but strong enough in content to create a diverse yet harmonious townscape in the Felton Village Core. The following is a summary of the consensus Design Guidelines developed during the fourth workshop:

Signage: preference was for serif style lettering on natural wood backgrounds

Facades: preference was for Western False Front Facades.

Building Materials: preference was for 1). natural wood, 2). painted wood, 3). redwood plywood with battens, 4). concrete block and natural stone. Prioritized from first priority to fourth.

Color and Paint:

- First choice. Buildings shall use natural materials and finishes, with color used as an accent only.
- Buildings may be painted; the major color should be a natural stain or hue with primary colors used only for trim and accent.

Sidewalks: preference for textured concrete or colored concrete in combination with regular concrete sidewalks.

Awnings: to be encouraged.

Lighting: energy efficient lighting constructed from natural wood was the first choice. There was a fear that "historic lamp posts" would be too cute or trendy looking. Lights should be of sturdy appearance and visually coordinated throughout the Village Core. Bollard lighting and pedestrian level lights shall be of redwood.

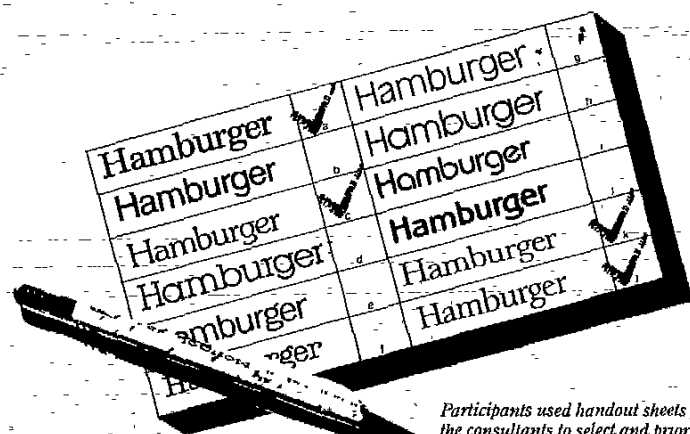
Merchant Questionnaire

Directly after the completion of Workshop #4, a questionnaire was compiled by the consultants and the Felton Merchants Association and hand distributed to all businesses and property owners in the Felton Village Core Area. The purpose of the questionnaire was to find out what improvements proposed during Workshops 2-4 were supported by area merchants and property owners, and to prioritize these improvements.

Thirty five questionnaires were returned 60% of the respondents had not been to any of the Felton Town Plan Workshops.

By far the top priority of those responding was the undergrounding of utilities on Highway 9. Following the undergrounding of utilities and listed in order of preference, was support for:

- Improvements to Graham Hill Road and to the Graham Hill Road/Highway 9 intersection.
- Coordinated street lighting on Highway 9.
- Coordinated seating and refuse containers on Highway 9.
- The Felton Commons.
- Protection of View Corridors.
- Signage Guidelines.



Participants used handout sheets prepared by the consultants to select and prioritize lettering styles for Design Guidelines.

Workshop #5

This workshop began with a slide show summary of the proposed Felton Town Plan improvements and an analysis of construction cost estimates for these improvements. A suggested list of small, "start-up" projects was discussed as well.

Lee Strong of *Downtown Focus*, a firm that specializes in working with business groups on the formation and management of assessment districts and business improvement districts and **Steve Costa**, president of *Bay Area Partnership*, an advocacy group that works on public/private sector partnerships in environmental design took part in a panel discussion with workshop participants.

Several issues were addressed during this discussion period as a result of panelists review of the proposed Town Plan and in response to questions raised by workshop participants.

- It was concluded that the proposed improvements were practical and could be implemented within 3-10 years.
- While the proposed improvements were unique to Felton, the scope of proposed improvements were similar to efforts in other small unincorporated towns.
- It is important to maintain the community effort and participation process generated during the Town Plan workshops through the implementation program.

- it is important to determine the group or agency responsible for achieving each of the proposed improvements
- it would be a good idea to find funding for a paid part time employee to help coordinate fund raising and other support strategies for the Town Plan improvements.

In addition to the discussion of these issues, the structure and functioning of business improvement districts and assessment districts was briefly outlined.

After the panel discussion, several workshop participants began to informally seek out other participants who would work on the organization of a Downtown underground utility district and research the possibility of bringing a farmer's market to Felton.

Farmer's Markets

Until recently, Farmer's Markets located in downtown areas have been frowned upon by local businesspeople, who viewed Farmer's Markets as competition which would draw away customers from local merchants. However, recent research has shown that just the opposite is true. Downtown merchants are now viewing Farmer's Markets as magnets to draw people downtown again, and reverse the flight to remote or suburban satellite super markets.

Downtown merchants are now reporting an increase in store sales from 8-14% with Farmer's Markets having a snowball effect on improvements in the immediate downtown area. Farmer's Markets are good for downtown! In San Luis Obispo, California the Saturday Farmer's Market (the largest in the state) is held in the parking lot of a local supermarket and on Thursday night another Farmer's Market is attracting customers to the downtown's "main street."

Farmer's Markets are helping to reverse the trend of people shopping in satellite centers, rather than in the downtown business district. Recent research conducted by the Center for Consumer Research at University of California in Davis has shown that:

- In Tracy, California's Farmer's Market, "More than half of the customers expected to tie in the trip (to the market) with other shopping, and they expected to spend an average of \$26 each at places downtown other than the farmer's market. . . . Another benefit of the market was its effect on people's impression of downtown Tracy—described as a more positive image by more than half of the customers interviewed."
- In Stockton, California's Farmer's Market, "Almost 90% reported that they ordinarily shopped outside the downtown area for their groceries. They expected to spend an average of \$10.50 at the market. Of those surveyed, 83% would not have come downtown on Saturday if not for the market. A third had tied in the trip with other activities, including shopping and dining. The amount that these people expected to spend at places downtown other than the farmer's market averaged \$14.50."
- "The positive effects of the farmer's markets can be seen in their (1) bringing additional people downtown, (2) having a direct economic benefit, and (3) causing favorable attitudes."

Based upon the location of "favorite places" identified by workshop participants and by an analysis of the pedestrian mapping of the Village Core, three potential Farmer's Market sites have been located on the Village site plan on the next page.

The locations were based upon: a) the availability of open paved area, b) availability of adjacent parking, c) existing pedestrian circulation as shown on the Pedestrian Maps on page 10.

Workshop participants who were interested in developing a Felton Farmer's Market have been meeting since the completion of the Town Plan workshops. They have obtained permission from a private property owner to hold a weekly Farmer's Market on the site adjacent to the Felton Branch Post Office on the west side of Gushee Street. Since June of 1987, the Felton Farmer's Market has taken place every Tuesday evening from 4-6PM. On August 2, 1987 in celebration of the restoration of the Felton Covered Bridge, a Farmer's Market and pancake breakfast was held on the Felton Covered Bridge.

Research on Farmer's Markets quoted in the Felton Town Plan was conducted by the Center for Consumer Research, Robert Sommer, Director. For more information:

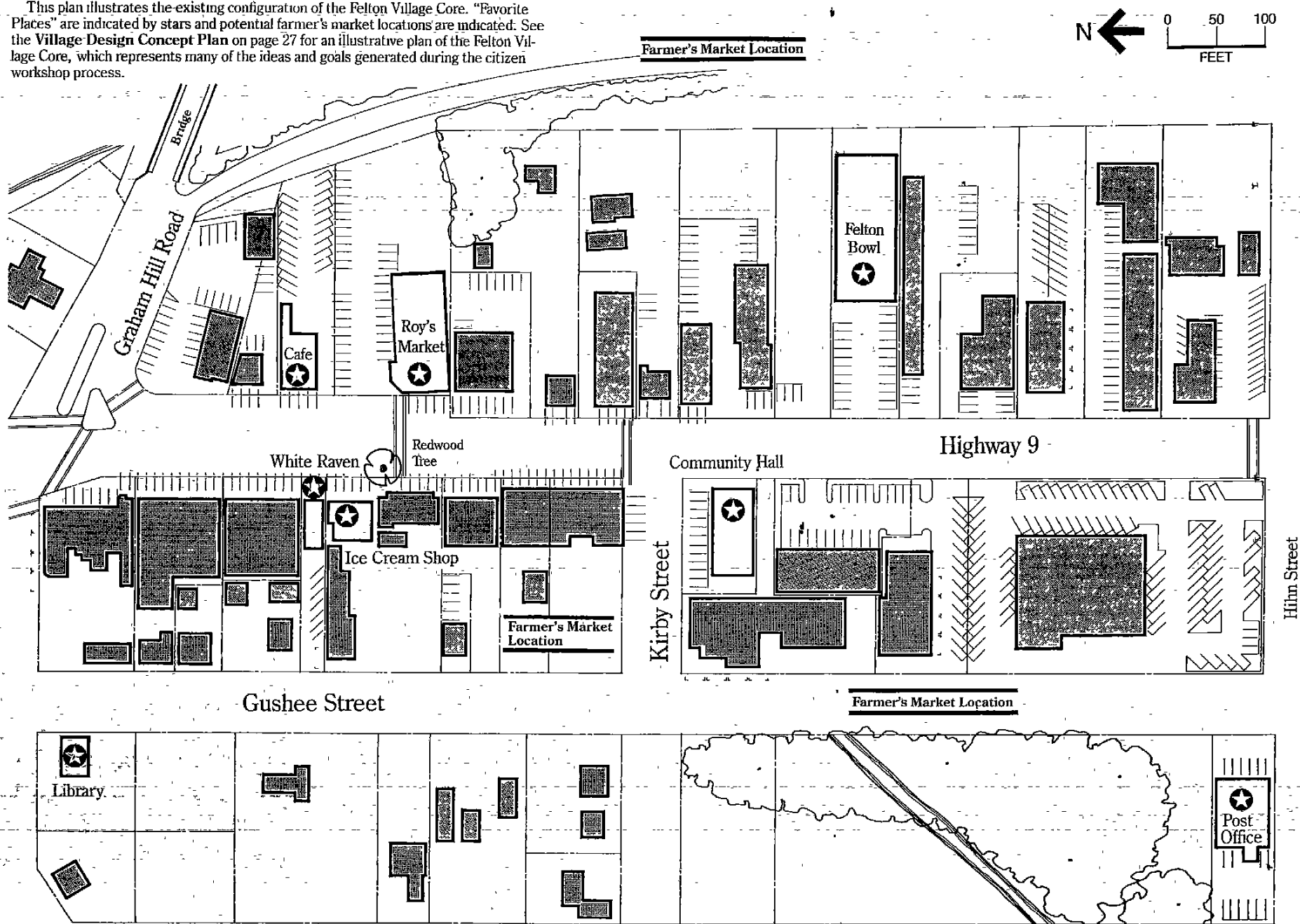
Robert Sommer, Director
Center for Consumer Research
148 Everson Hall
University of California, Davis
Davis, California 95616



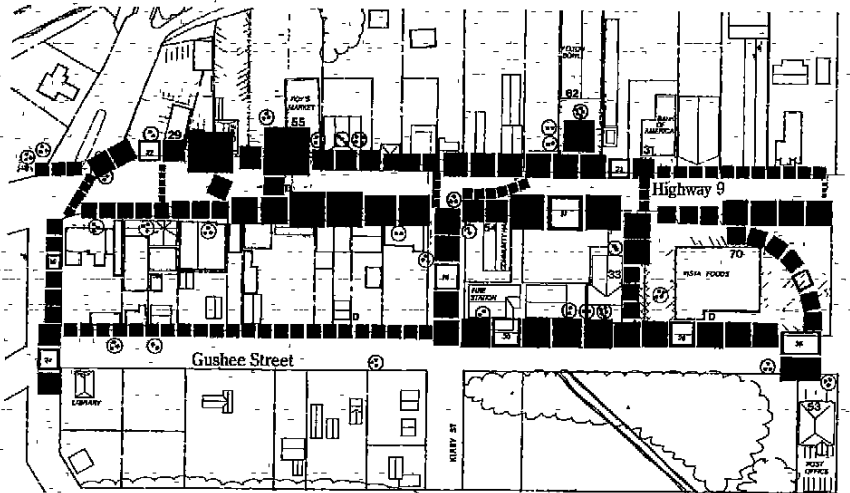
Felton's new Farmer's Market.

Existing Felton Village Core

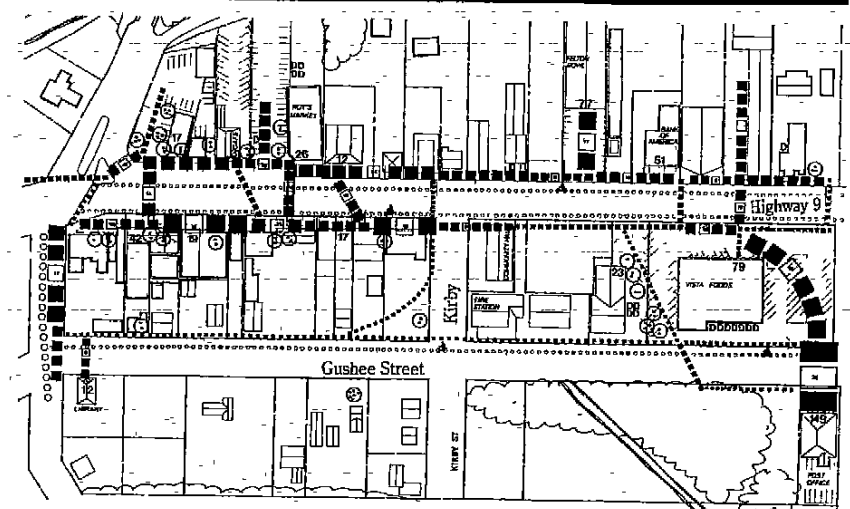
This plan illustrates the existing configuration of the Felton Village Core. "Favorite Places" are indicated by stars and potential farmer's market locations are indicated. See the **Village Design Concept Plan** on page 27 for an illustrative plan of the Felton Village Core, which represents many of the ideas and goals generated during the citizen workshop process.



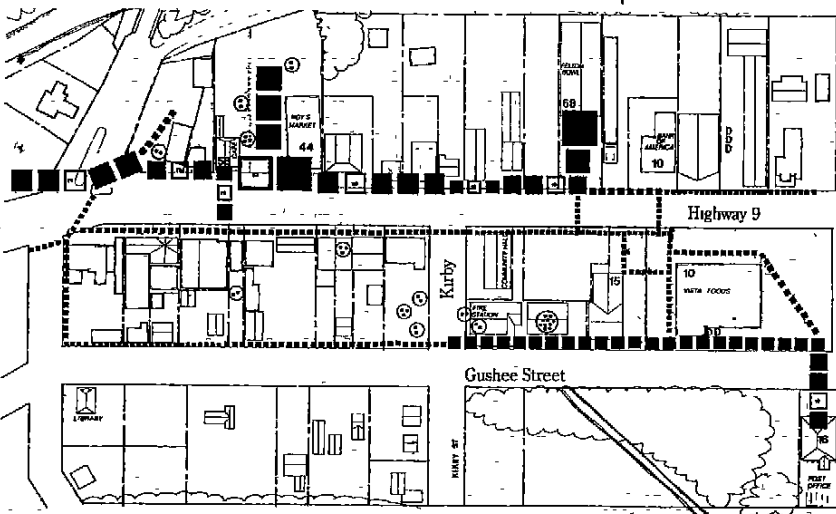
Pedestrian Mapping



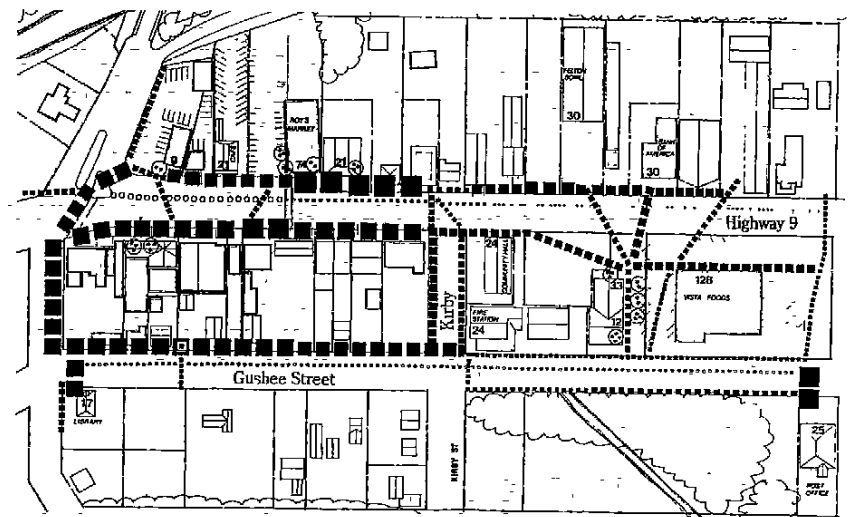
Saturday 12-1PM



Weekdays 12-1PM



Saturday 9-10AM

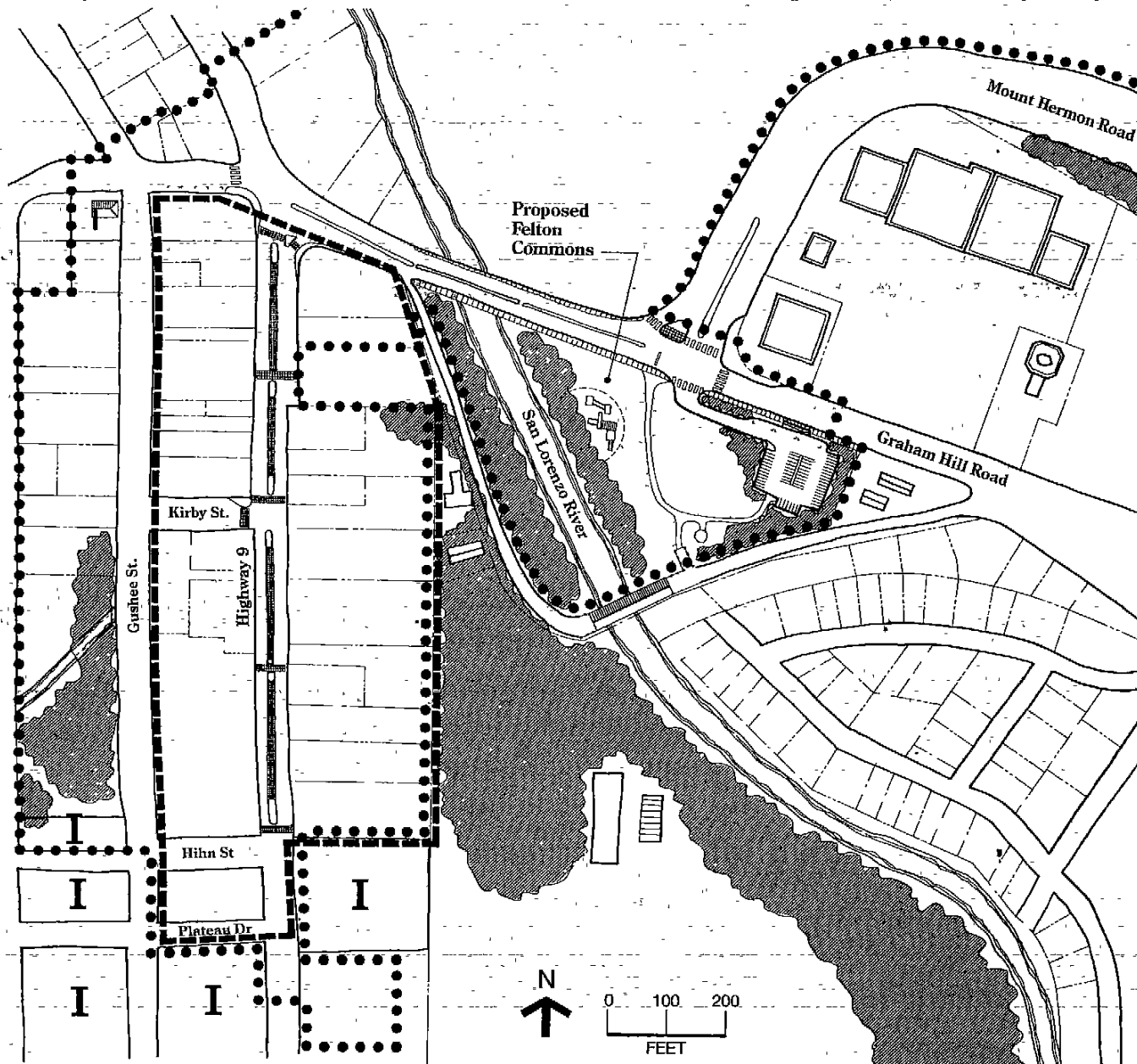


Weekdays 4-5PM

Pedestrian Maps of the Felton Village Core were created by trained observers and illustrate the location and intensity of pedestrian uses throughout the Village at various times of the day and week. Circles with solid black dots within them represent observed conversations. Numbers within a building indicate how many people entered that building within a one hour time period, and the letter "D" indicates a delivery vehicle.

The Pedestrian Maps were used by Workshop Participants during the participatory design process and were helpful in locating: pedestrian crosswalks, bus stop locations, landscaping, lighting, and the potential location of farmer's markets

Community Commercial District



Felton Underground
 Utility District - No. 10

Community Commercial
 - - - - - District Boundaries

I Institutional

The objective of Community Commercial Centers according to section 6.2.2 of the Santa Cruz County General Plan is:

- to provide Community Commercial areas with definite boundaries such that the concentration of public and private investment and planning efforts will result in more viable commercial areas which serve a role as special places for working, shopping and cultural activities and which increase the efficient use of transportation.

It is the objective of the Felton Town Plan to focus future development and revitalization in the Community Commercial District illustrated on the map to the left. Expansion of the Community Commercial Center is allowed under General Plan Policy 6.2.8 but should only be allowed when the Community Commercial Center is unable to accommodate additional commercial activity through:

- infill development concepts as illustrated in this plan
- second story expansion of existing structures, or
- redevelopment of existing structures.

Policy:

- 1) The Community Commercial Center boundaries shall be established as illustrated on the map to the left.
- 2) The expansion of Community Commercial uses beyond these boundaries shall not occur until such time as the Community Commercial District can no longer support additional commercial activity through:
 - infill development as illustrated in this plan
 - second story expansion of existing structures
 - redevelopment of existing structures, or at such time when parking and/or sanitation requirements preclude further development within the proposed Community Commercial Center.
- 3) Redesignate and Rezone the west side of Gushee Street to Residential, with Bed & Breakfast Inns and small scale Professional Offices allowed as Conditional Uses.

Underground Utility District

Utility District

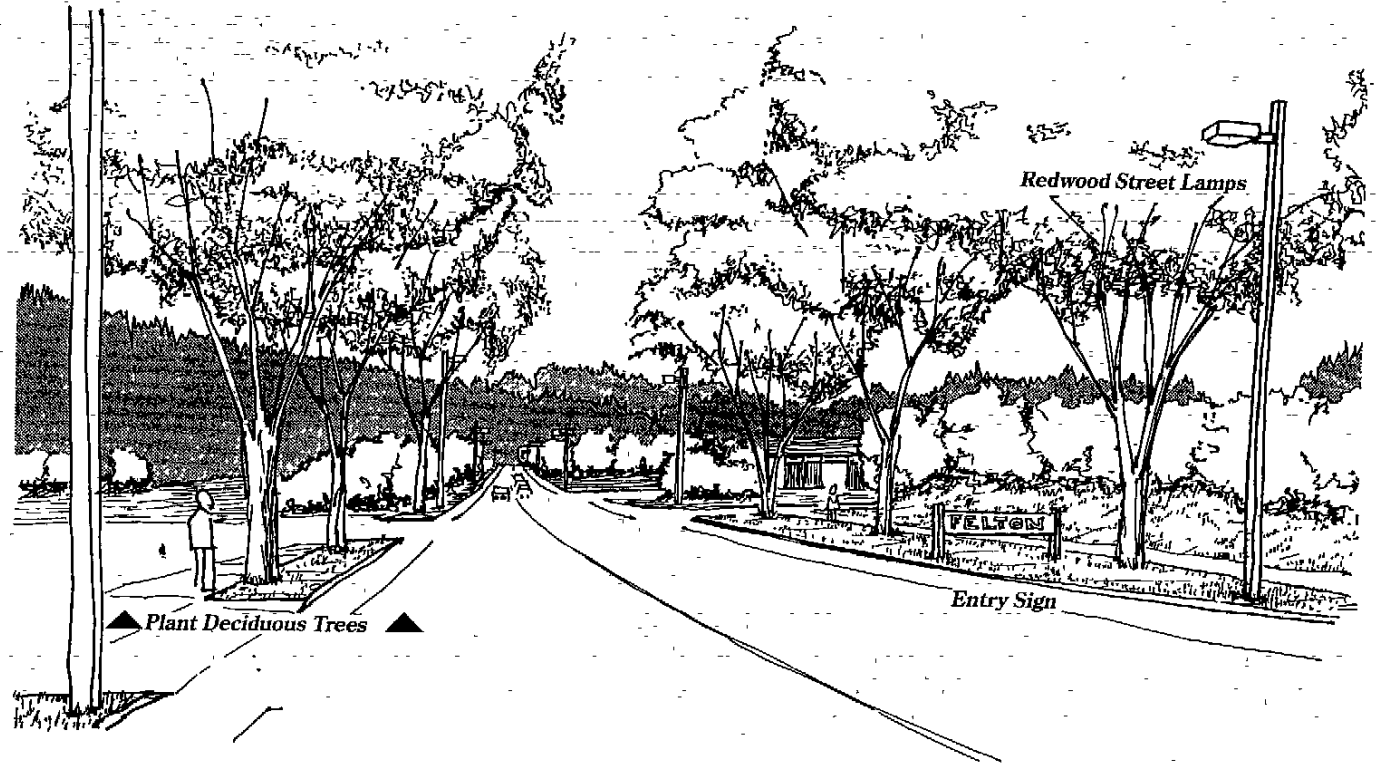
In order to improve the visual appearance of the Felton Village area, workshop participants were unanimous in their support of an underground utility district. Furthermore, a merchant/property owner questionnaire distributed by the Felton Merchants Association garnered over 70% support for such a district.

A district for undergrounding of all PG&E, cable and telephone utilities is illustrated on the map to the left and includes:

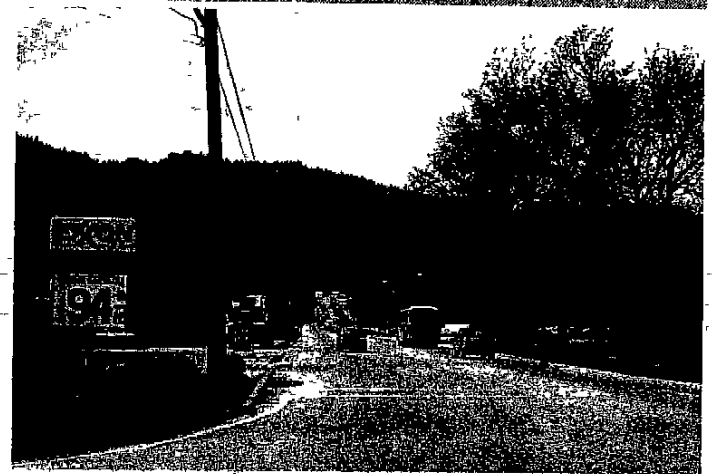
- *Graham Hill Road from Covered Bridge Road South to Gushee Street to be incorporated with proposed road improvements (see page 23), and to enhance: a) entry to Felton and b) views of the Felton Covered Bridge and the Scenic View Corridors described on pages 13 to 17)*
- *Covered Bridge Road North; underground or remove Pacific Telephone cable line in front of west entry to Felton Covered Bridge*
- *Highway 9 from Plateau Drive to Graham Hill Road to enhance the visual character of the Village Core*
- *Gushee Street from Plateau Drive to Felton Empire Grade, and*
- *utility undergrounding for all new buildings, major renovations and planned developments within the Community Commercial Boundaries.*

As a result of the Felton Town Plan community workshop process, a renewed interest in the undergrounding of utilities in the Felton area occurred. Workshop participants organized themselves and began a petition drive throughout the Felton Village Core. The petition drive, organized with the help and support of the Felton Business Association gathered petitions in support of the Underground Utility District, including signatures from over 70% of the property owners who would be included in the proposed District.

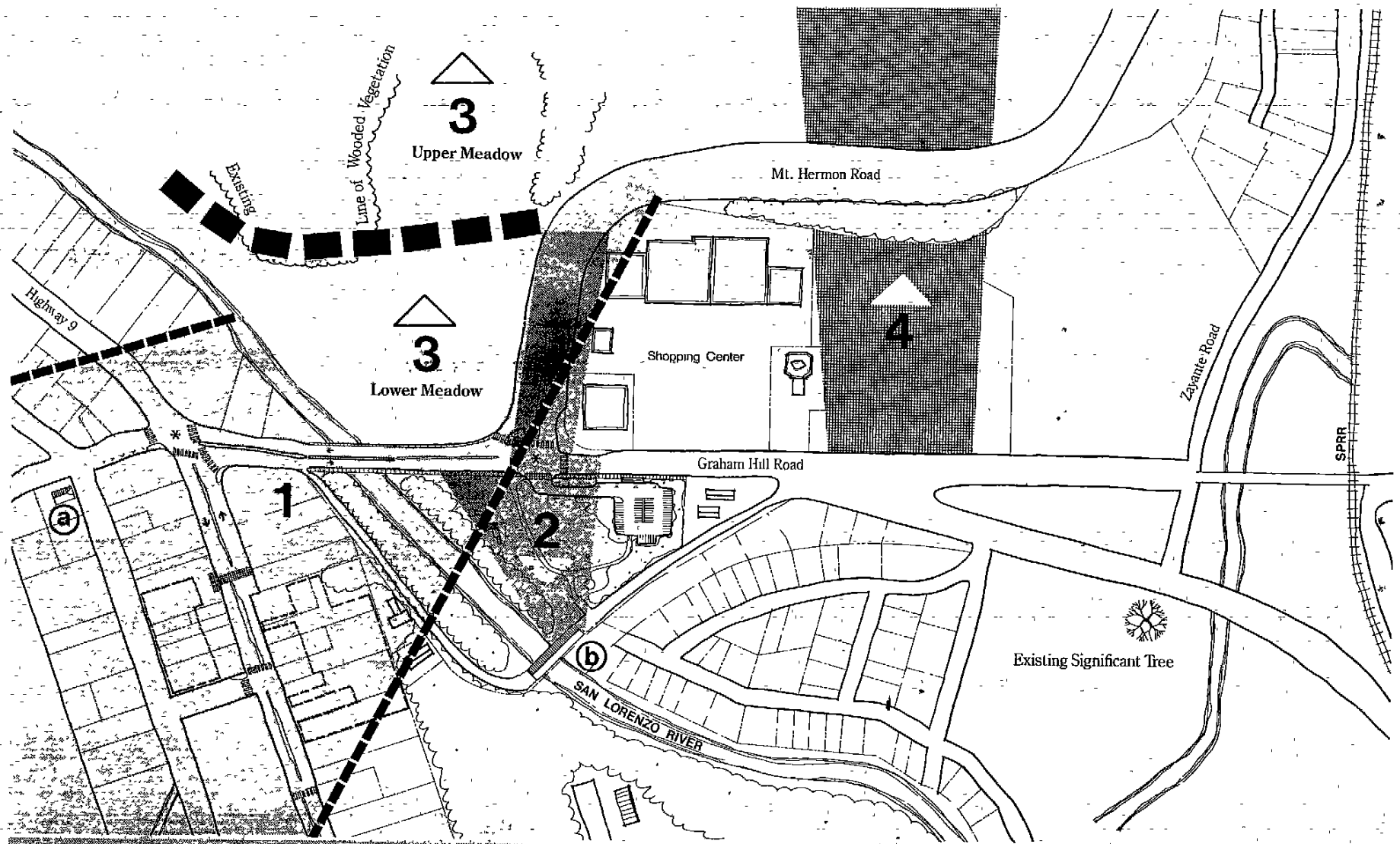
On February 25, 1986 the Santa Cruz County Board of Supervisors approved the formation of the Felton Underground Utility District - No. 10, the boundaries of which are illustrated on the map to the left.



Entering Felton from Graham Hill Road



Scenic View Corridors Map



Listed in the National Register of Historic Places:

a. View Corridor #1: Faye Bilardi Memorial Building

b. View Corridor #2: Felton Covered Bridge

Scenic View Corridor #1

Scenic View Corridor #2

Approximate Division Between Upper and Lower Meadow in Scenic View Corridor #3

Scenic View Corridor #4

Direction of View Shed

Scenic View Corridors

Scenic View Corridor Guidelines

Purpose

To preserve and protect: 1) views of structures listed in the National Register of Historic Places, 2) views of the natural environs forming the scenic backdrop for the Felton Village area, 3) views that frame the sense of entry to the Felton Village area, and 4) areas of visual importance that provide scenic entry to the Felton Community Commercial District.

The Felton Covered Bridge and the Faye Bilardi Memorial Building are listed in the National Register for Historic Places and are focal points in the Felton townscape. Both structures have been designated as *favorite places* during the Felton Town Plan workshop process.

Guidelines

All development within the four Scenic View Corridors must conform to the Felton Town Plan Townscape/Design Guidelines and conform to the specific requirements of the Scenic View Corridor in which the proposed development is located.

Scenic Corridor Designation

Given the importance of Felton Village "entryways" and scenic vistas of the Felton Village Core described by participants in the Felton Town Plan Workshop process, it is recommended that Graham Hill Road and Mount Hermon Road be included in the Santa Cruz County General Plan, Scenic Corridor Designation.

Recommendation:

The County should designate the following County Roads as Scenic Highways as per Santa Cruz County General Plan Section 1.7.1.2.:

- Mount Hermon Road, from Lockwood Lane to Graham Hill Road, and
- Graham Hill Road, from Ocean Street to Highway 9, in Felton

Scenic View Corridor #1

Purpose

To require new development within Scenic View Corridor #1 to harmonize with existing scenic and historic vistas of the Felton Village, and to preserve the integrity and scale of the **Faye Bilardi Memorial Building** (listed in the National Register of Historic Places), as seen from locations east of the Village including Mount Hermon Road:

Scope

Two areas of concern are identified:

- a). Development within the Village Core, as defined by Graham Hill Road, Gushee Street, Plateau Drive and the San Lorenzo River.
- b). Development on land west of Gushee Street, including sloping hillsides which are visible from and within Scenic View Corridor #1.

Definitions

- a). **New development** shall mean new buildings and/or significant additions to, or remodels of existing buildings.
- b). **Harmonize** shall mean employment of design and construction techniques, building scale and massing, colors and materials which comply with the Felton Town Plan Design Guidelines and which compliments the existing architectural composition of the Felton Village area.

Guidelines

- a). Village Core
 1. New development in the Village Core shall conform to the Felton Town Plan Design Guidelines and shall complement the existing village architecture in terms of scale, height, proportion, massing and architectural style.
 2. No buildings or structures shall be constructed which block views of the Faye Bilardi Memorial Building, as seen from Mount Hermon Road entering the Village, or which otherwise impact or adversely compete with the view of the Bilardi Building.
- b). West of the Village Core; development west of Gushee Street and located on sloped hillsides and which is visible from View Corridor #1 shall

1. be designed to conform to existing land forms and topography, via terracing or other design or construction techniques which mitigate and reduce the perceived height of new development, as seen from Scenic View Corridor #1.
 2. be designed to screen substantial portions of the bulk and height of new development from having a dominating effect upon the Scenic View Corridor.
 3. utilize natural materials and colors as defined by the Felton Town Plan Design Guidelines.
 4. not dominate by the nature of its scale, massing, design or location the function of the Faye Bilardi Memorial Building as a focal point and entry landmark to the Felton Village.
- a). Mount Hermon Road
 1. Vegetation in the Mount Hermon Road right-of-way shall be managed to preserve views as described and illustrated in these Scenic View Corridor Guidelines.

Application Requirements

In addition to any other permit review requirements which may apply, applicants may be

required to furnish any or all of the following, in the course of the review process.

- A. An accurate topographic survey completed by a licensed Land Surveyor or Civil Engineer.
- B. Cross-section of the proposed site illustrating proposed cut and fill and indicating proposed building heights.
- C. An expanded elevation or photocollage illustrating the proposed structure in relationship to the Scenic View Corridor and indicating the precise scale and location of landmark structures in the immediate impacted area.
- D. Where proposed structures might impact or block vistas of the Faye Bilardi Memorial Building as seen from Mount Hermon Road, a cross section shall be submitted which illustrates existing view angles and ensures that proposed structures do not block or impact existing views.
- E. Data produced in Application Requirements B, C and D shall be based upon the Topographic Survey required in A above.

Scenic View Corridor #1 as seen from Mount Hermon Road entering Felton



Scenic View Corridor #2

Purpose

To preserve the visual importance, historic appearance and sense of entry to the Felton Village area provided by the Felton Covered Bridge. The bridge, built in 1892, is believed to be the tallest covered bridge in the United States and is listed in the National Register of Historic Places. The Felton Covered Bridge has been identified as a *favorite place* by participants in the Felton Town Plan Workshop process.

Guidelines

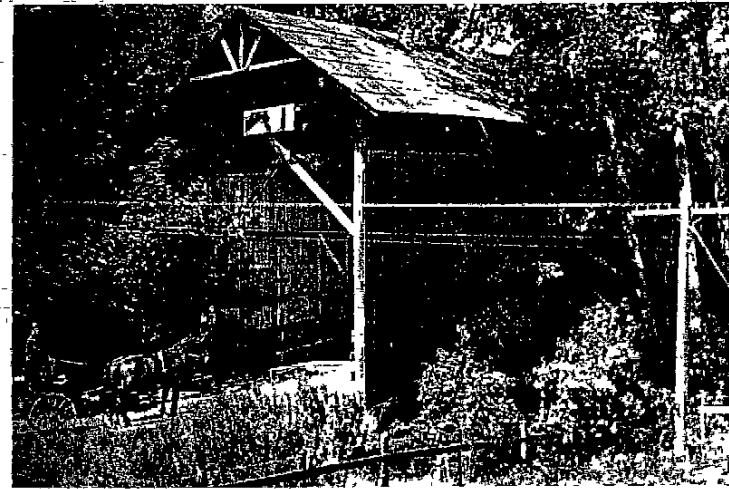
These guidelines are for all proposed buildings, structures, signage, utility infrastructure, landscaping and all light standards and public safety signage located in Scenic View Corridor #2.

- 1) New or renovated physical improvements or landscaping shall not block or limit views or visually distract from, or compete with, views of the historic Felton Covered Bridge, as seen from Graham Hill Road or from Mount Hermon Road as illustrated in the Scenic View Corridor #2 overlay on page 13.

- 2) Signage, traffic signalization and utilities as required to protect the public safety and welfare shall conform to the General Design Guidelines in this Plan and shall be designed and constructed to maximize and enhance public views of the historic Felton Covered Bridge.
- 3) Trees and vegetation within Scenic View Corridor #2, including the proposed Felton Commons and existing trees adjacent to the Felton Covered Bridge shall be maintained in order to enhance views of the Covered Bridge.

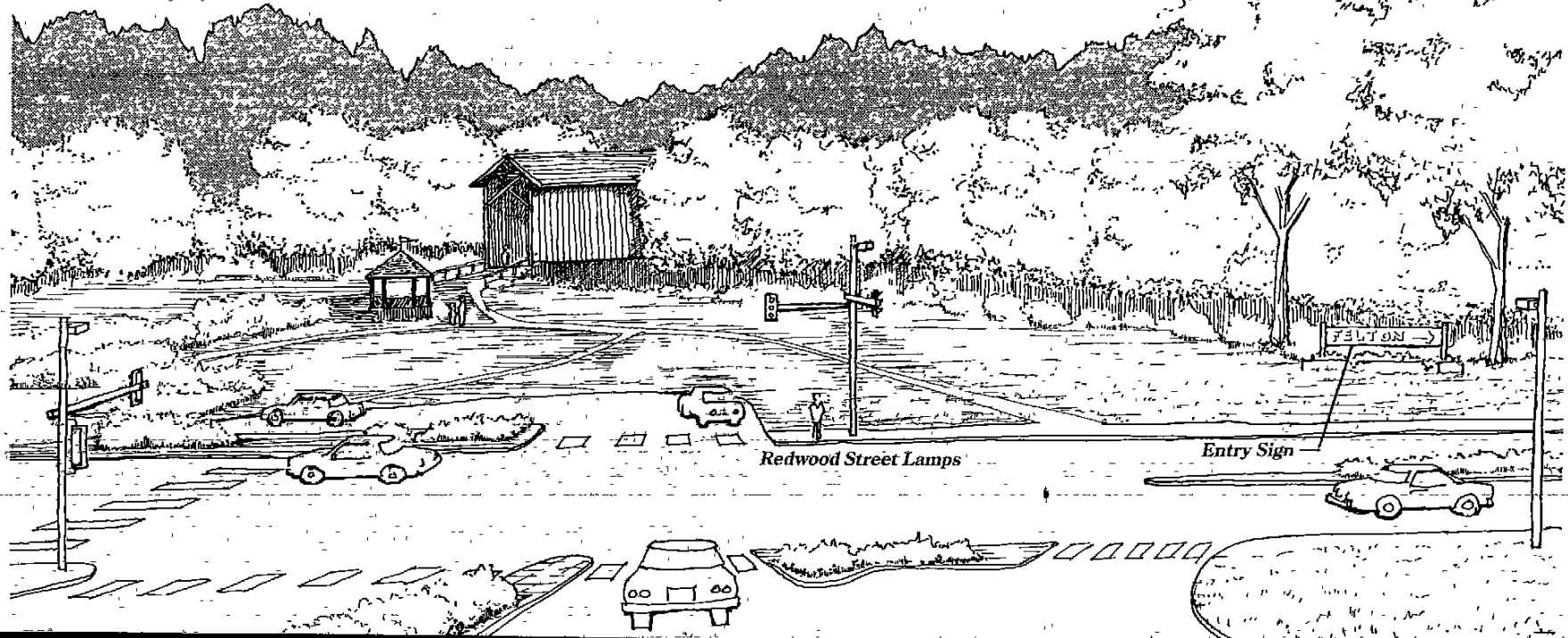
Application Guidelines

- 1) Applications shall illustrate in plan and elevation the location of proposed structures and other physical improvements (as listed in the Guidelines above) indicating their relationship to the Felton Covered Bridge viewed.
- 2) Elevations of proposed improvements to the public right of way on Graham Hill Road and Mount Hermon Road shall indicate the height and scale of proposed improvements in relationship to views of the Covered Bridge.



Oldest known photograph of the Felton Covered Bridge shows the appearance of the Covered Bridge in 1908.

Entering Felton from Mount Hermon Road



Scenic View Corridor #3

Purpose

To preserve the natural visual appearance of the existing open meadow and the hills and landscape features north of the meadow as an entryway to the Felton Village area, and as a buffer in the event of future development on the remaining portion of the parcel north and west of the view shed.

Location

As illustrated on the Scenic View Corridor overlay #3 on page 13, and bordered by Graham Hill Road to the south, Mount Hermon Road to the east, and the San Lorenzo River to the west

Guidelines

1) **Area of minimum disturbance:** The lower open meadow portion of the area in View Corridor #3 shall be preserved in its natural open condition through the siting of all buildings, structures, and active uses on other portions of the property except as provided herein. Grading and changes to the existing meadow vegetation and natural appearance shall be strictly minimized.

Agricultural, animal husbandry, recreational and open space uses, and ancillary uses in support of a primary development located elsewhere on the property, may be allowed in this area if they involve minimal building and site disturbance, preserve the natural open space character of the site, and conform to the Felton Town Plan General Design Guidelines.

2) **Allowable Building Area:** Buildings and other structures and uses may be allowed on the upper meadow portion of Scenic View Corridor #3 if fully screened from view by naturalized landscaping which is harmonious with the existing meadow and landscape.

The allowable development area shall be located north of the existing line of wooded vegetation extended eastward to meet the woodland extending down to the bend in Mount Hermon Road. Siting of development shall be based upon a professional visual analysis and evaluation of the site to maintain the natural character of the setting.

3) Transfer of Density:

Development which would be allowed under Santa Cruz County Plans and Ordinances but precluded by these Scenic View Corridor Guidelines, shall be transferred to other developable portions of the site beyond the visual limits of the lower open meadow and existing landscape buffer.

4) **Access Improvements:** Roads, pathways and other rights of way developed in the Scenic Corridor shall be so constructed and located as to be visually inobtrusive or buffered and screened from view, and shall be of materials and design that blend with the surrounding landscape and rural setting, and conform to the Felton Town Plan General Design Guidelines.

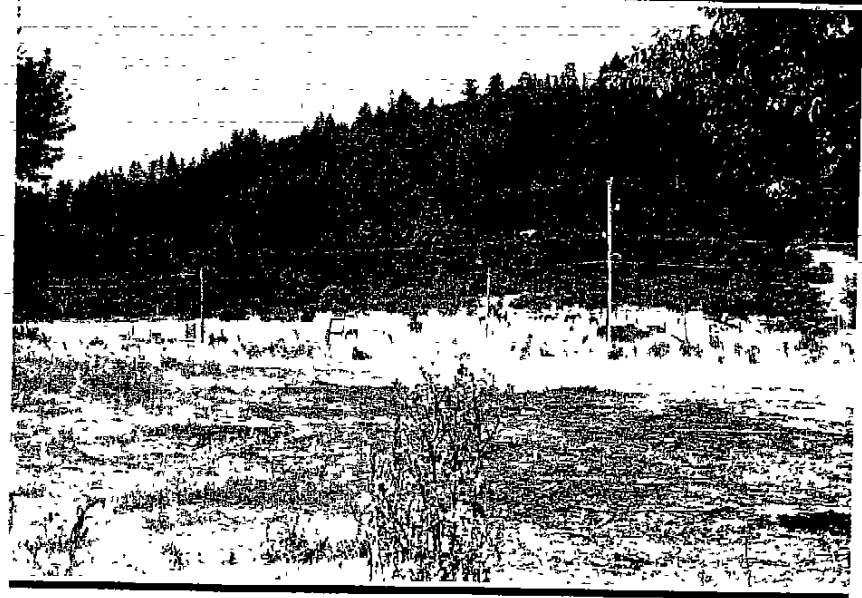
The County shall cooperate in the provision of adequate access to the meadow property while insuring safe traffic circulation on Mount Hermon Road and the protection of the visual character of the area.

Application Requirements

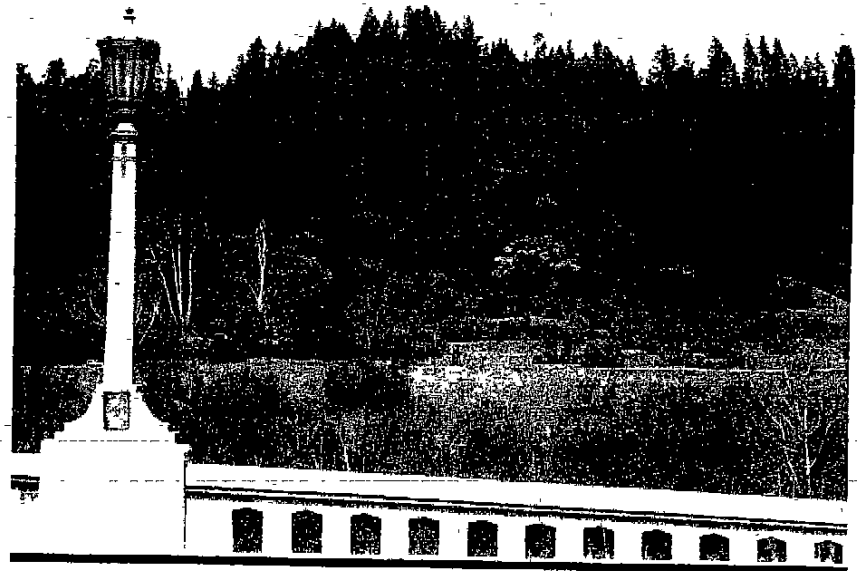
Applications shall meet all existing Santa Cruz County application requirements, the General Design Guidelines of this Plan, and:

1) Provide a Topographic Survey completed by a licensed Land Surveyor or Civil Engineer that illustrates existing land contours and elevations, location and type of vegetation, the FEMA flood plain and flood way elevations, and other natural features of the site (Refer to the Federal Emergency Management Administration—FEMA—Flood Plan Map on page 21)

2) Provide topographic cross sections of the site from Graham Hill Road north to the existing hills, indicating 30% slope and proposed buildings, structures, roads and pathways as well as detailed landscaped screening of these proposed improvements.



Scenic View Corridor #3 viewed from the proposed Felton Commons site, south of Graham Hill Road



Scenic View Corridor #3 viewed from the concrete bridge on Graham Hill Road

Scenic View Corridor #4

Purpose

To preserve the scenic views and natural visual setting provided by the hills north of Mount Hermon Road and adjacent to and south of Mount Hermon Road as viewed from Graham Hill Road and the Felton Grove vicinity.

Location

As illustrated on the Scenic View Corridor overlay #4 on page 13, and bordered by County Bank to the west, Graham Hill road to the south and Mount Hermon Road to the north.

Guidelines

The Guidelines are for all proposed buildings, structures, parking areas, landscaping and signage within Scenic View Corridor #4.

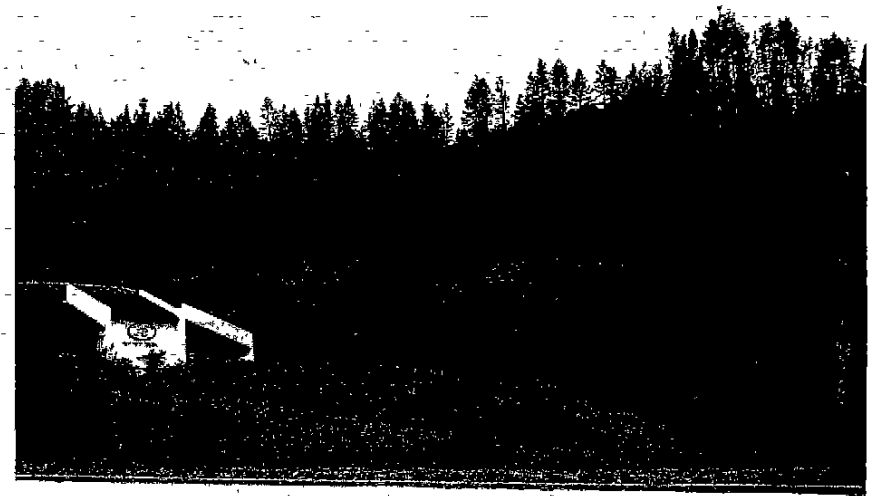
- 1) **Allowable Building:** All proposed structures and other development within Scenic View Corridor #4 shall use design and construction techniques which maintain a low profile and do not interfere with, block or otherwise distract from the view shed. Acceptable design or construction techniques include
 - a. building forms which conform to existing topography or are integrated with existing land topography in such a manner that views of the hills northeast of APN 71-331-05 are not obstructed.
 - b. partial below grade construction, or
 - c. building forms screened by berms and/or landscaping.
- 2) **Transfer of Development Rights:** Development which would normally be allowed under Santa Cruz County Plans and Ordinances may be transferred to the adjoining parcel directly east of Scenic View Corridor #4. Parking and access could occur within the view shed if buffered as described in Item #1 above.

Application Requirements

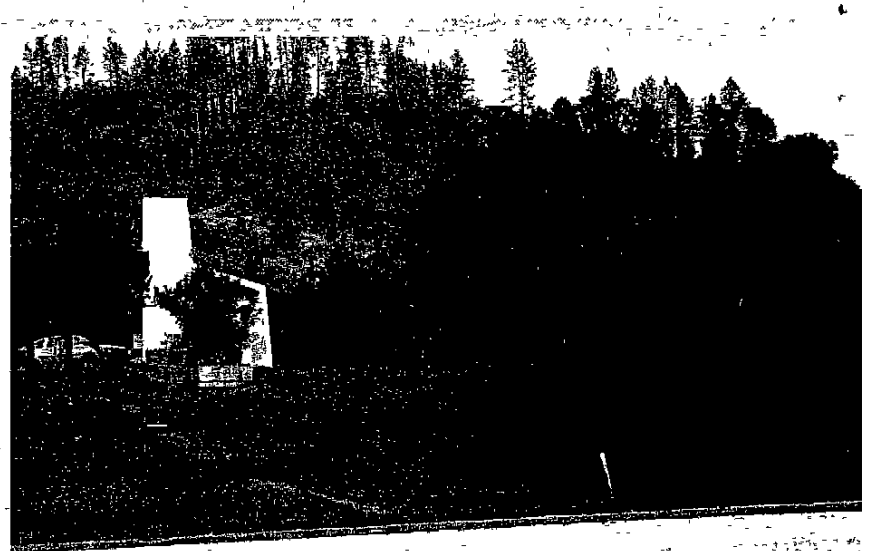
Applications shall meet all existing Santa Cruz County application requirements, the General Design Guidelines in this Plan, and:

- 1) Provide a Topographic Survey completed by a licensed Land Surveyor or Civil Engineer with land contours and elevations, location and type of vegetation, and indication of slopes of 30% or greater

- 2) Provide a cross section from Graham Hill Road to Mount Hermon Road illustrating (to scale) proposed structures and parking areas and methods used to preserve the view shed. All finish materials shall be indicated on the drawings.
- 3) Item #2 above shall include an illustrated (to scale) line of sight from Graham Hill Road to Mount Hermon Road and shall demonstrate that said line of sight is free from all obstructions and that the view shed is maintained.
- 4) A landscape plan shall indicate all landscape materials and features and their mature heights and widths and their impact upon the view shed.

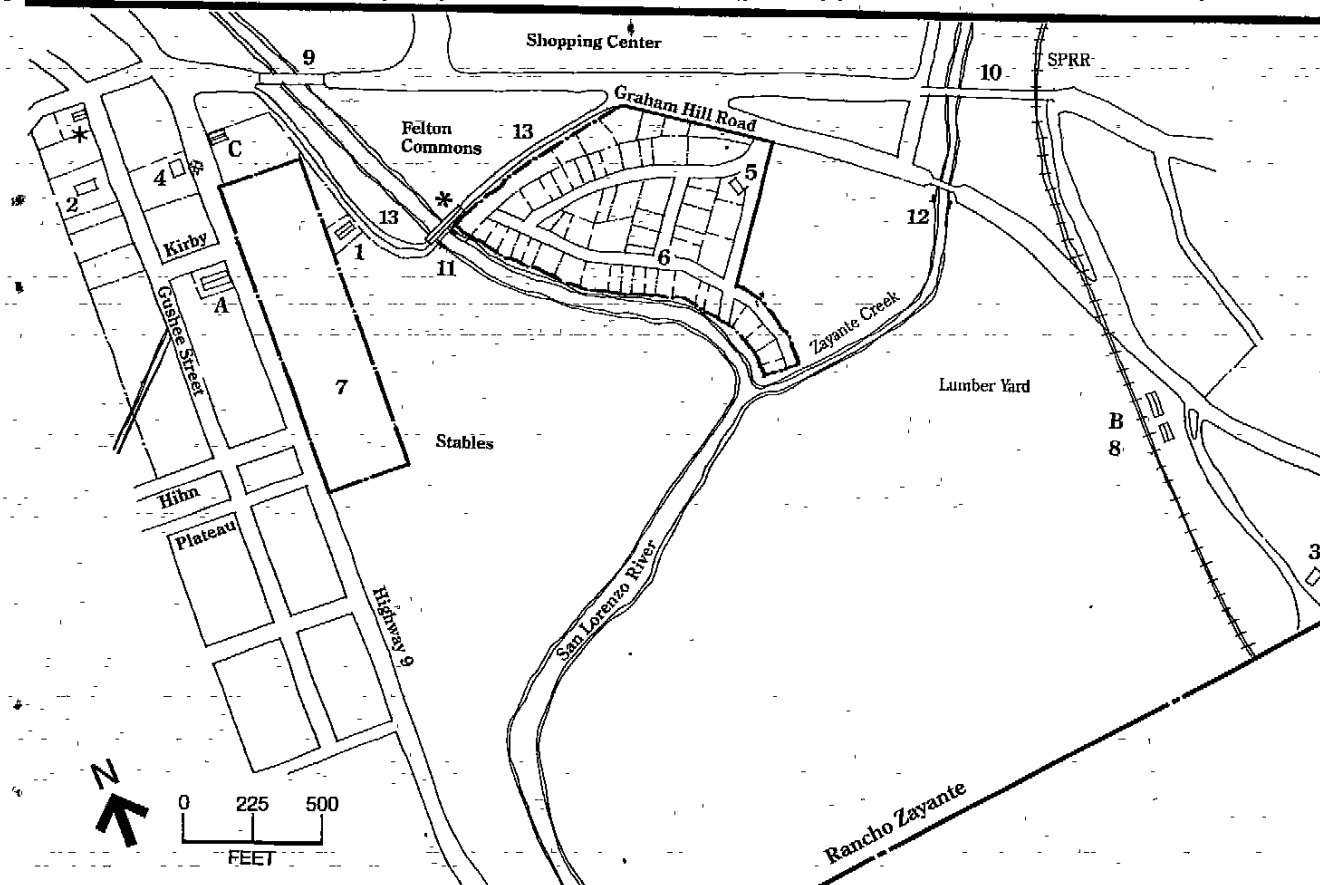


Scenic View Corridor #4 viewed from Graham Hill Road, with County Bank to the left in the photograph



Scenic View Corridor #4 viewed from Graham Hill Road, with County Bank to the left in the photograph.

Historic Resources



Historic Resources identified and correlated with Recommendations discussed below.

The history of the built environment in Felton is a rich and colorful one. Railroads, lime kilns, lumber, and covered bridges all contributed to this rich background. Historically, Felton was the transportation center of the San Lorenzo Valley.

Unfortunately, major fires have destroyed all but a few of the significant buildings in the Felton Study Area. Without a fabric of historic structures which create a recognizable "townscape," it is doubtful that the Felton Village Area could be considered for National Historical District status. However, Felton's history remains a significant part of living and working in the Felton area. Of the top six favorite places identified during the

community workshop process, three are historic structures. Furthermore, visitors from all over the world travel to Felton to visit the Felton Covered Bridge and ride the historical steam trains at Roaring Camp.

The following is a list of significant historic locations and structures in the Felton Village Study Area. The Felton Covered Bridge and the Faye Bilardi Memorial Library are listed on the National Register of Historic Places. Additions to the County Historic Resources List are recommended below, and were compiled as a result of a one day field survey conducted by members of the County Historic Resources Commission, Planning staff and the Felton Town Plan consultant team:

National Register of Historic Places

- * Felton Covered Bridge (see page 19)
- * Faye Bilardi Memorial Library

Santa Cruz County Historic Resources Inventory

- A Felton Community Hall
- B Felton Railroad Depot at Roaring Camp
- C Kramer House (now Heavenly Cafe)

Recommended Additions to the County Historic Resources Inventory

1. Blacksmith Shop northwest of the Felton Covered Bridge (APN 065-081-05)
2. Residence at 6259 Gushee Street (APN 065-074-15)
3. Residence at Roaring Camp (possible the original Rancho residence)
4. Building now Schmiede's Plumbing, believed to be built in the late 1800's and moved to this site. Further research is required.
5. Auto Court Building in River Grove at the apex of Circle Drive and Park Avenue (APN 065-143-01)

6. Felton Grove Autocourt Subdivision
7. South Pacific Coast Railroad Company Depot area; original property line boundaries.
8. Felton Junction Area, site of Felton Railroad Depot at Roaring Camp
9. Graham Hill Road Concrete Bridge and Lamp fixtures, replaced Covered Bridge as Felton entry circa 1937.
10. Graham Hill Road Concrete Bridge, crosses Zayante Creek, circa 1935.
11. Railroad abutments south of the Felton Covered Bridge. (See photograph below)
12. Railroad abutments south of Graham Hill Road/Zayante Creek Bridge (1917).
13. Covered Bridge Road north and south; original access road to Felton.

Policy:

Refer the list of recommended additions to the Historic Resources Inventory to the County Historic Resources Commission for detailed documentation, and initiation of a General Plan amendment for inclusion in the County Historic Resources Inventory.



Felton Covered Bridge with the railroad trestle located just south of the Bridge—circa 1908.

See Also:

- Favorite Places pg 5
- Scenic View Corridors pg 13
- Felton Covered Bridge pg 19
- Library pg 37
- Townscape Elevation of Kramer House pg 38

Felton Covered Bridge

The Felton Covered Bridge, built in 1892-93 by the Cotton Brothers of Oakland, is listed on the National Register of Historic Places and believed to be the tallest covered bridge in the United States. The bridge was used as a vehicular bridge until 1937, when it was replaced by the concrete bridge on Graham Hill Road. Images of the Felton Covered Bridge are used as symbols for many organizations throughout the San Lorenzo Valley and in general the covered bridge is the most recognized symbol of the San Lorenzo Valley.

However, since approximately 1940, the covered bridge has been in disrepair. Volunteers from the Felton area have maintained the bridge utilizing funds raised during annual pancake breakfasts. During the flood of 1982, a giant redwood tree headed downstream collided with the bridge deck. Although the bridge withstood the collision fairly well, Federal funds became available and restoration of the historic Felton Covered Bridge began.

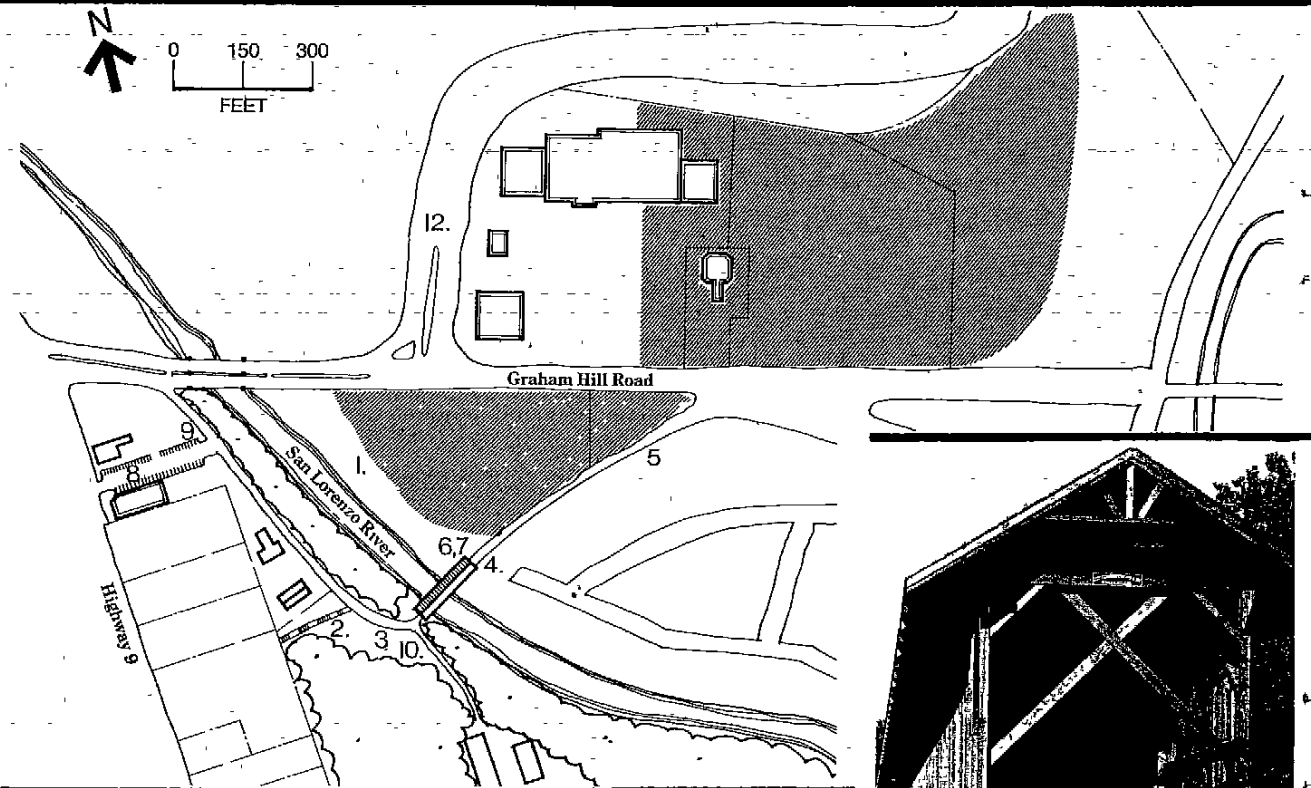
The restoration of the Felton Covered Bridge was completed in three phases, with the last phase completed in January 1987.

- Phase 1: Repair and restoration of bridge deck, entry ramps, entry ramp foundations and west entry support posts and bracing.
- Phase 2 Streambank stabilization to prevent the undermining of non-reinforced, concrete abutments.
- Phase 3: Restoration of wind girders, siding, roof framing and roof shakes.

There are three inter-related policy areas intended to further preserve and enhance the Felton Covered Bridge:

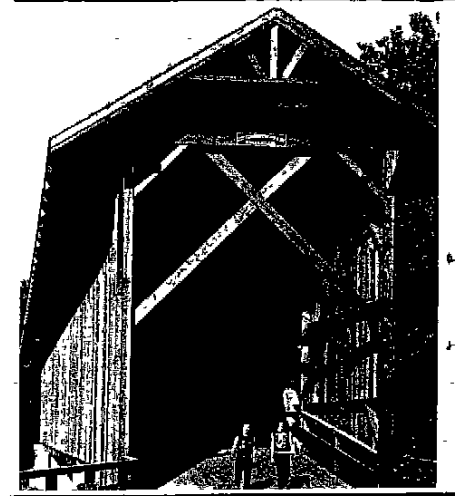
- **Increase pedestrian use of the Covered Bridge, improve pedestrian safety and decrease vandalism, by a continuing program of Bridge repair and maintenance and the addition of night lighting.**

1. Develop the Felton Commons area for parking, community activity and historic displays.
2. Install stairs linking Covered Bridge Road with the Felton Bowl area (an informal pathway already exists there).
3. Provide benches on the west side of the bridge.
4. Provide lighting at the east and west entry areas.



The Felton Covered Bridge Vicinity with the grey area illustrating rainfall catchment area impacting the Covered Bridge. Numbers correspond with the policies on this page.

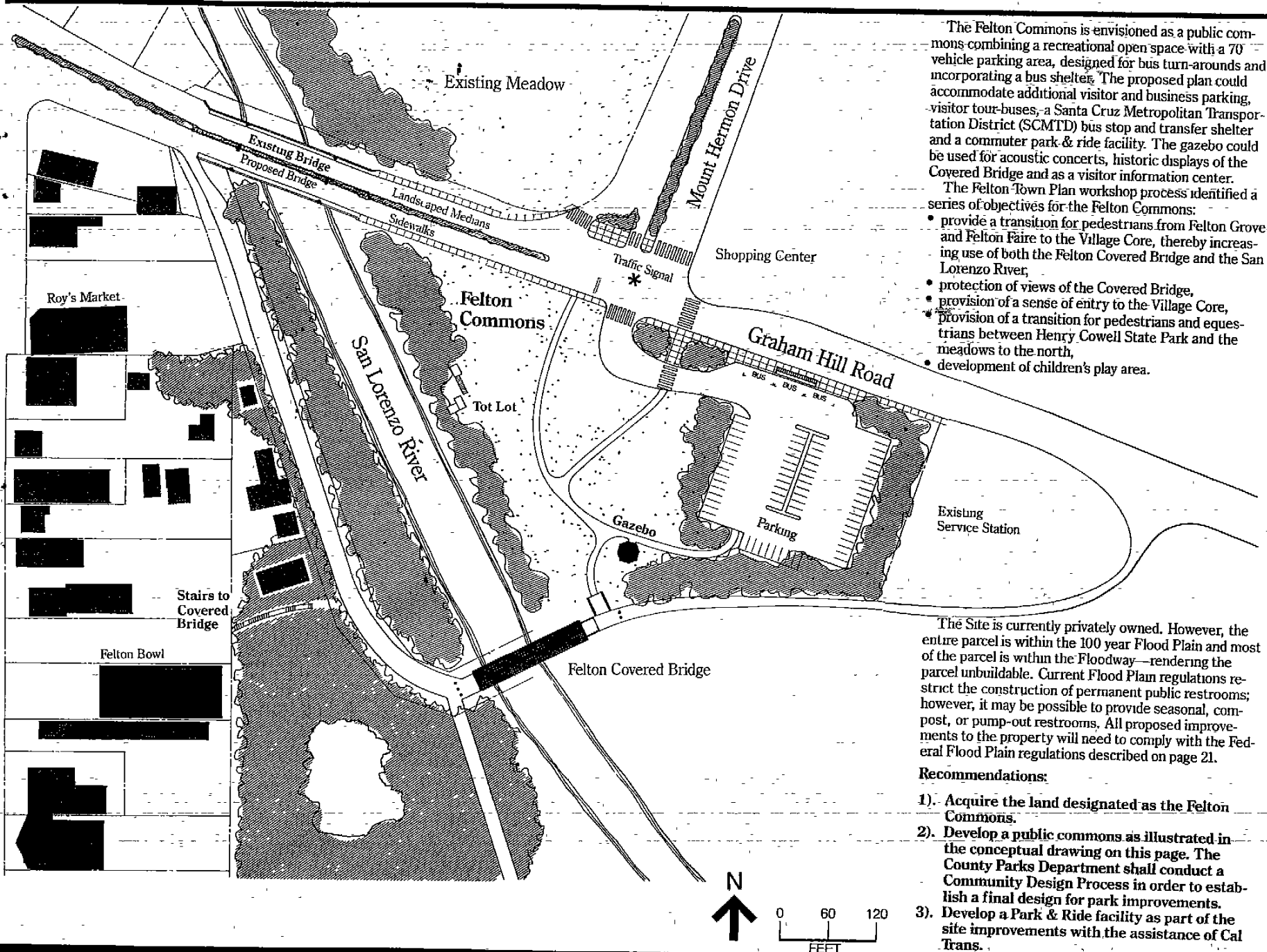
- **Protect the bridge abutments from further undermining by strictly enforcing existing County policies concerning on-site retention of rain runoff. Rain runoff from the area north-east of the covered bridge currently flows to the San Lorenzo River through a culvert just north of and adjacent to the east bridge abutment. The rainfall runoff catchment area is illustrated by the grey tone on the map above.**
- 5. During the implementation of the Felton Commons Plan substitute a natural drainage swale for the existing below grade culvert on Covered Bridge Road South.
- 6. Replace the existing undersized culvert adjacent to the covered bridge with an extension of the open swale discussed above.
- 7. Install a wood pedestrian bridge connecting the east ramp of the Covered Bridge with the Felton Commons and crossing the open swale drainage area.
- **Develop a stronger connection between the Felton Covered Bridge and the Felton Village. Encourage out of town visitors to park in the Village and walk to the Bridge.**
- 8. Paint a mural of the Covered Bridge on the north side of Roy's Market.
- 9. Identify parking for the Covered Bridge and pedestrian paths to the Bridge.
- 10. Landscape the area adjacent to the west entry to the Covered Bridge both to prevent parking and to make the area more enjoyable and safe for pedestrians.



Felton Covered Bridge, west entry—photograph taken June 1987, after completion of restoration

11. Explore an alternate southern access road to the stables, and use Covered Bridge Road North for residential vehicular access only.
12. Enforce a Scenic View Corridor Policy which protects and enhances views of the Covered Bridge.
13. Explore the feasibility of a horse drawn wagon connecting Roaring Camp and the Felton Village Core via the Covered Bridge.
14. Remove or underground telephone wires and pole in front of the west entry to the Covered Bridge.

Felton Commons



The Felton Commons is envisioned as a public commons combining a recreational open space with a 70' vehicle parking area, designed for bus turn-arounds and incorporating a bus shelter. The proposed plan could accommodate additional visitor and business parking, visitor tour-buses, a Santa Cruz Metropolitan Transportation District (SCMTD) bus stop and transfer shelter and a commuter park & ride facility. The gazebo could be used for acoustic concerts, historic displays of the Covered Bridge and as a visitor information center.

The Felton Town Plan workshop process identified a series of objectives for the Felton Commons:

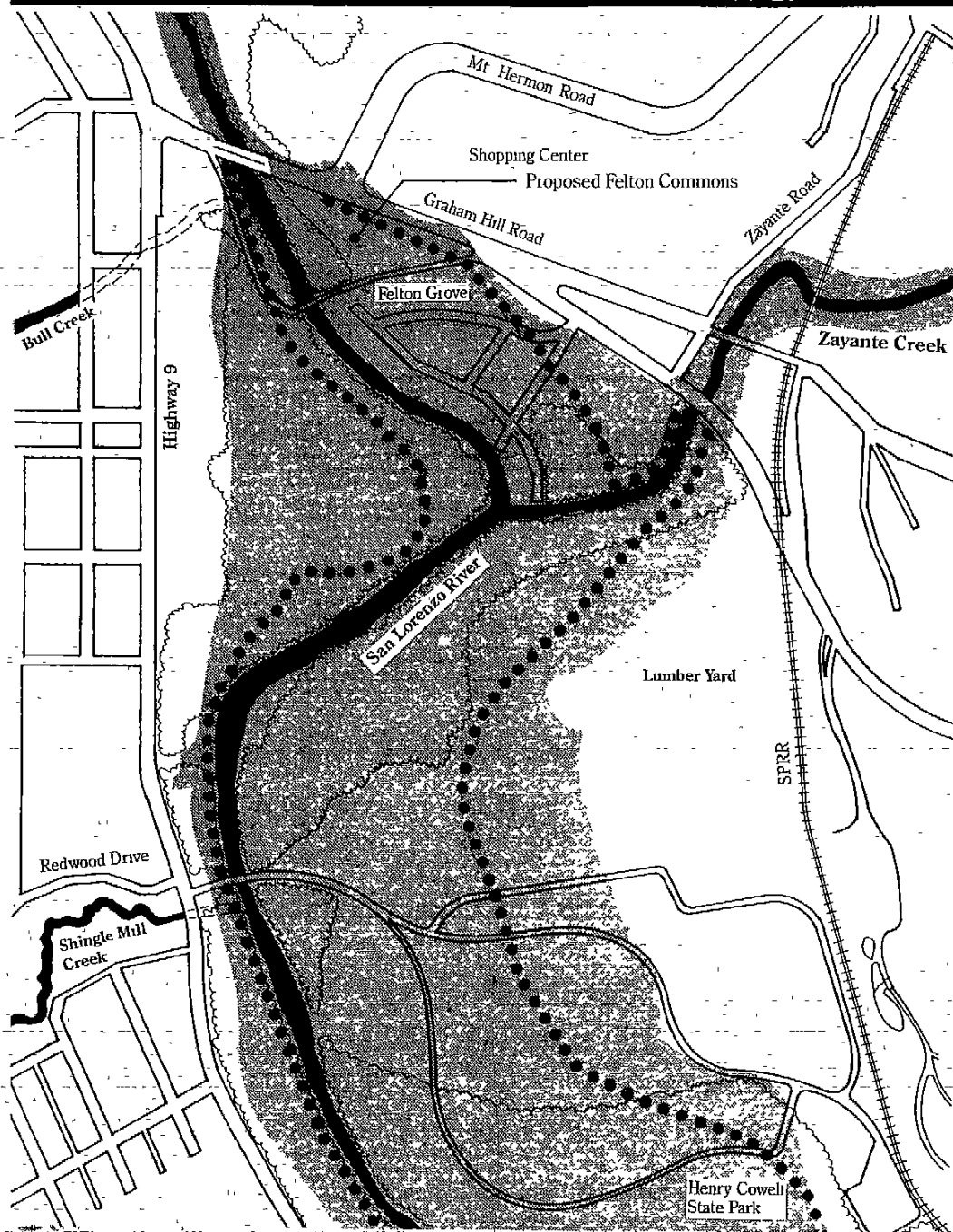
- provide a transition for pedestrians from Felton Grove and Felton Faire to the Village Core, thereby increasing use of both the Felton Covered Bridge and the San Lorenzo River;
- protection of views of the Covered Bridge,
- provision of a sense of entry to the Village Core,
- provision of a transition for pedestrians and equestrians between Henry Cowell State Park and the meadows to the north,
- development of children's play area.

The Site is currently privately owned. However, the entire parcel is within the 100 year Flood Plain and most of the parcel is within the Floodway—rendering the parcel unbuildable. Current Flood Plan regulations restrict the construction of permanent public restrooms; however, it may be possible to provide seasonal, compost, or pump-out restrooms. All proposed improvements to the property will need to comply with the Federal Flood Plain regulations described on page 21.

Recommendations:

- 1). Acquire the land designated as the Felton Commons.
- 2). Develop a public commons as illustrated in the conceptual drawing on this page. The County Parks Department shall conduct a Community Design Process in order to establish a final design for park improvements.
- 3). Develop a Park & Ride facility as part of the site improvements with the assistance of Cal Trans.

Flood Plain Map



Discussion

Flooding in the San Lorenzo River Basin is primarily a result of an excess in precipitation, either in intensity or over a prolonged period of time. Because the river basin is fairly small and very steep, flooding is often due to a combination of the intensity of rainfall and the length of time of rainfall. Flood flows usually occur within a few hours of peak rainfall, and are not usually above damaging levels for more than 18-36 hours. The main flood season in the San Lorenzo River Basin is from November thru April, when 90% of annual rain falls.

It has been estimated from newspaper accounts that major flooding has occurred along the San Lorenzo River 12 times in the past 107 years, or on the average, every 9 years. In the past twenty years, floods have occurred in 1969, 1973, 1978 and 1982. In each case, the area proposed for the Felton Commons and the Pelton Grove Area (see map to the left) were flooded. In 1982, the Felton Covered Bridge was damaged during a flood, which observers claim, rose to approximately three feet above the Bridge deck. In 1986, the Felton Grove area was flooded again and flood waters caused stream bank erosion around the Felton Covered Bridge, which resulted in major additional damage to the Bridge.

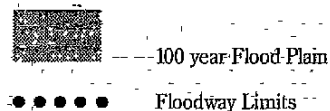
Felton Town Plan workshop participants and County Public Works staff concur that flooding is also a serious problem in the Village Core area, adjacent to Highway 9, and is impacted by undersized Highway 9 culverts and by rainfall runoff from Bull Creek. This area is not within the 100 year Flood Plain charted by FEMA, however Cal Trans has agreed to correct the flooding problem in this area.

Flooding is also a problem at the intersection of Graham Hill Road and Mount Hermon Road (which is within the Flood Plain). This problem is expected to be alleviated in combination with road improvements proposed at this intersection and being undertaken by Cal Trans.

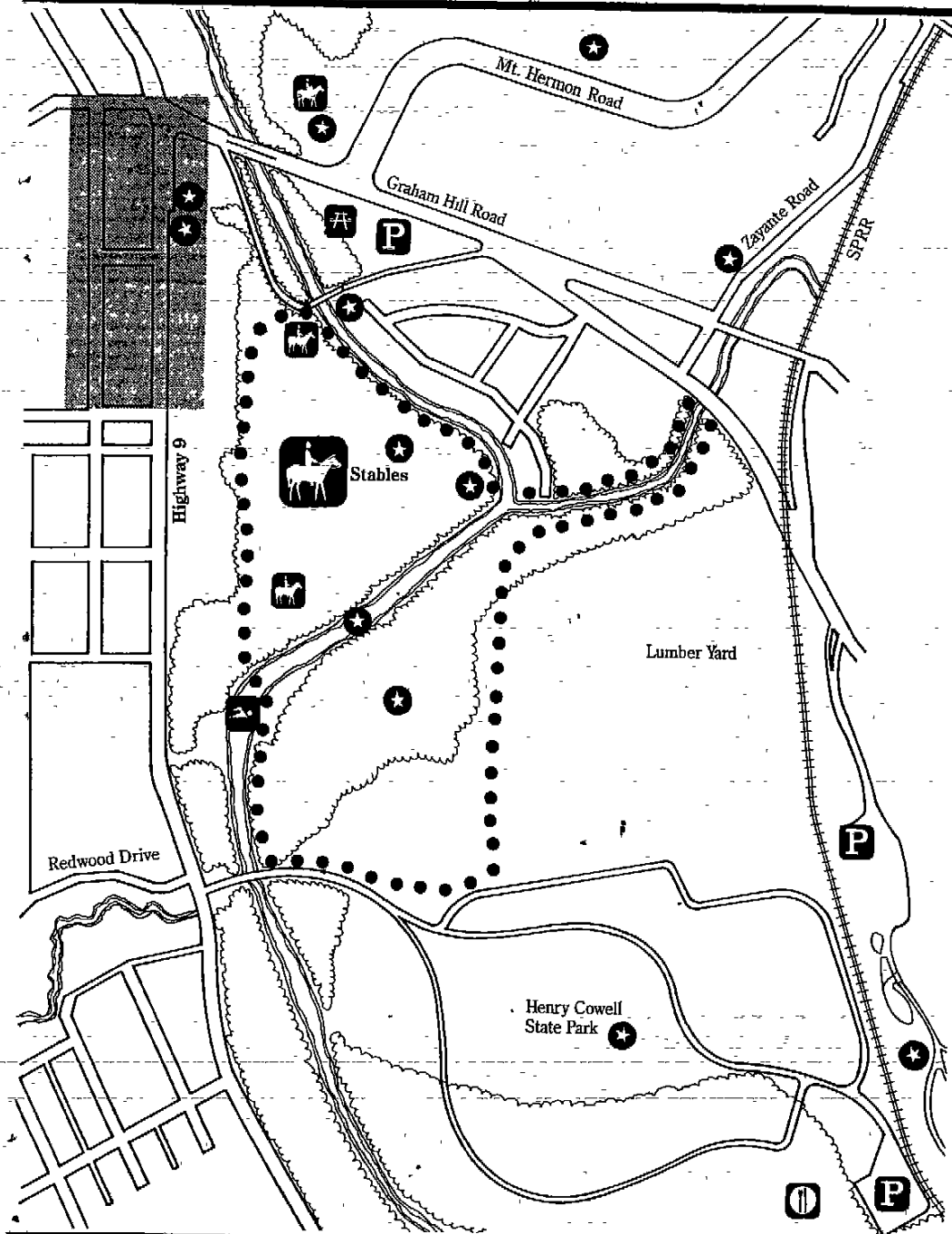
Building restrictions in the Flood Plan include a) no septic systems are allowed, b) new residences must be elevated, and c) new commercial structures must be floodproofed. Furthermore, new development activity is not allowed in the Floodway.

The Floodway is an extremely hazardous area due to the quantity and velocity of flood waters, the amount of debris which may be transported and the high potential for erosion in this special flood hazard area during periods of large stream flows. Therefore, all development activity, except for the reconstruction, alteration or improvement of an existing structure is prohibited within the Floodway unless exempted by State or Federal laws.

Note This map illustrates the approximate location of the 100 year Flood Plain and the boundaries of the Floodway. Please refer to FEMA Flood Plain maps on file with the County Planning Department for detailed planning information. (FEMA = Federal Emergency Management Administration)



Open Space/Recreation



An assessment of the *favorite places* and *favorite experiences* of people in Felton revealed three attributes of the Felton area that people who live and work in Felton felt essential:

- historic buildings and structures
- face to face contact and friendliness of townspeople
- use and enjoyment of open space.

Of the favorite places, Henry Cowell State Park was the top vote getter. Fall Creek Park, the San Lorenzo River, and the meadows north of the Felton Commons were all named favorite places.

Of the favorite experiences, 6 of the 7 activity oriented favorite experiences involved open space activities and were located within the Study Area boundaries illustrated by the map on this page. The location of these favorite places/favorite experiences are indicated by a star on the map to the left and include:



- walking, running and jogging in Henry Cowell State Park
- horse back riding from Zayante Road to the Felton Covered Bridge and to Roaring Camp, and
- swimming, walking, playing in the San Lorenzo River.

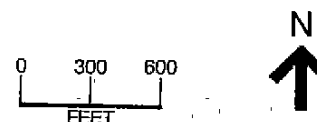
The open space and natural areas in the Felton Study Area are located in an approximate north/south orientation following the general direction of the San Lorenzo River. Henry Cowell State Park is located at the south end of the Study Area. A large, "natural" area within the flood plain (see solid black dots) is informally used for hiking and equestrian use. Stables are located in this area, just south of the Felton Covered Bridge.

The proposed Felton Commons which is envisioned as a public commons creates a transition between the open areas to the south and the meadows north of Graham Hill Road.

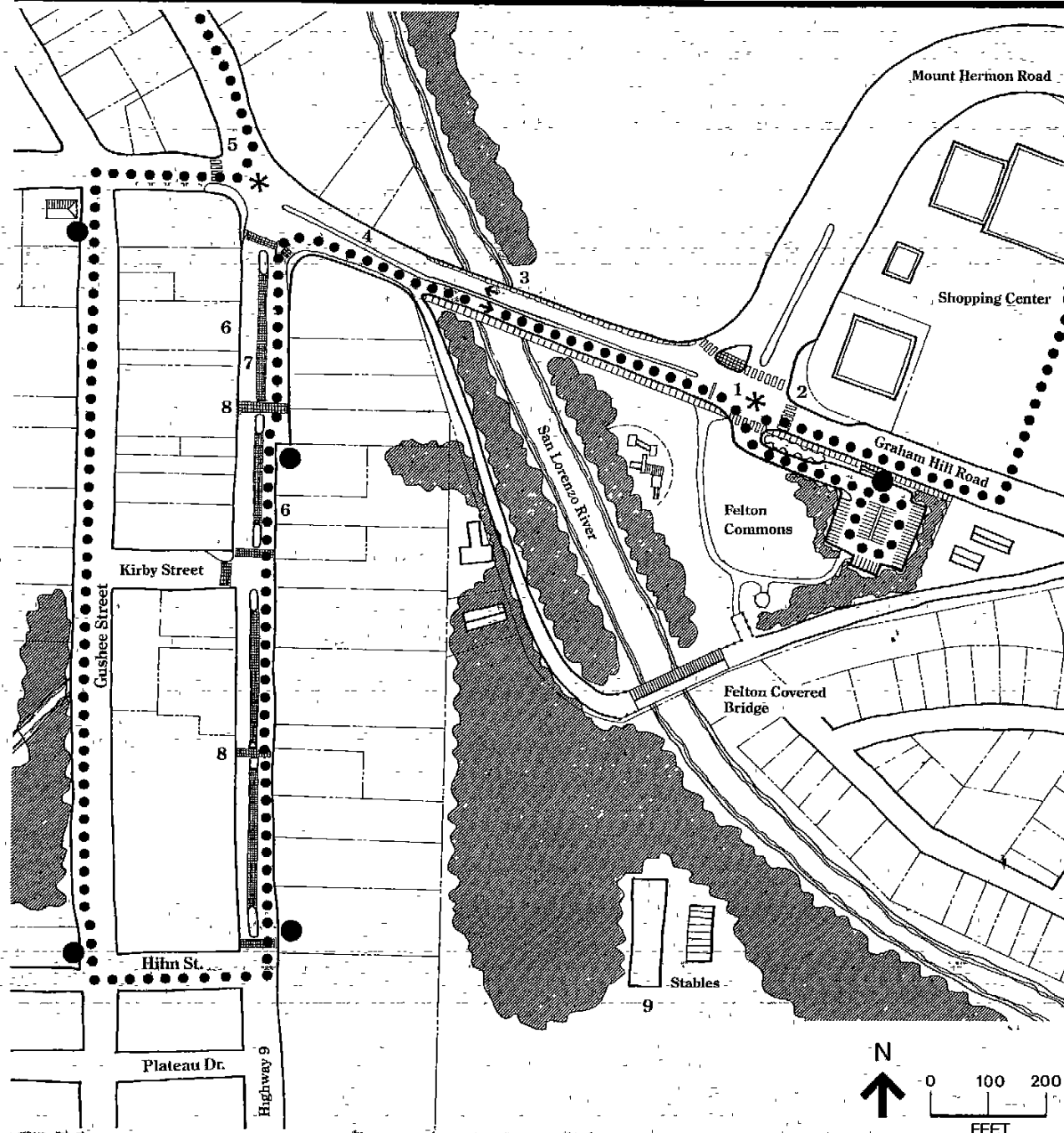
Open Space Policies.

- 1). Maintain the privately owned equestrian stables south of the Felton Covered Bridge and ensure its continued availability to the community.
- 2). Obtain public trail easements for pedestrian and equestrian use along the San Lorenzo River and Zayante Creek and inter-connecting the stables, Felton Covered Bridge, Felton Commons, Felton Village Core and open space areas indicated on the map to the left.
- 3). Support public purchase (using County and/or State funding) of the Flood Plain Area, if necessary, to maintain the recreational uses currently enjoyed by the Felton Community.

	Village Core	See Also.	
		• Scenic View Corridors	pg 13
		• Felton Covered Bridge	pg 19
		• Felton Commons	pg 20
		• Flood Plain Map	pg 21
		• Circulation Map	pg 23
	Favorite Places		



Circulation Improvements Map



Legend for Proposed Circulation and Parking Improvements

1. Signalize the intersection of Graham Hill Road and Mount Hermon Road.
2. Provide a pedestrian curb island (bulb-out) at the Graham Hill Road/Mount Hermon Road intersection as a pedestrian safety measure.
3. Maintain the existing concrete bridge on Graham Hill Road for two-lane westbound travel and add an additional two-lane bridge for eastbound travel, south of the existing bridge. Sidewalks and bicycle lanes should be an integral part of the bridge re-design.
4. Provide raised landscaped medians on Graham Hill Road.
5. Provide double left turns southbound onto Graham Hill Road from Highway 9.
6. Change the existing 90 degree parking on Highway 9 to angled parking.
7. Provide a continuous left turn lane of textured pavers on Highway 9 from Hihn Street to Graham Hill Road. Install raised landscaped, median islands at the ends of the continuous left turn lanes. See pages 29-30 for specific Design Plans.
8. Install pedestrian crosswalks of textured pavers, as illustrated.
9. Investigate a southern access route to the existing stables.

● Proposed SCMTD Bus Stop location.

●●●● Initiate an SCMTD Bus Loop service in the Felton Village Core as illustrated.

Note See the section on Highway 9 Design and Circulation Criteria on Page 35

Circulation and Parking

Graham Hill Road/Mt. Hermon Road Intersection

The Graham Hill Road/Mount Hermon Road intersection is a "T" intersection with approaches from the east, west and north and a proposed approach south from the Felton Commons. Currently, three of the turning movements at the intersection are controlled by signage. Mt. Hermon Road is a four lane road which is divided as it approaches Graham Hill Road. Graham Hill Road is a two lane road with a left turn pocket for eastbound traffic and an acceleration lane for traffic turning left from Mt. Hermon Road.

Conditions in the area have combined to exceed vehicle capacity of the intersection at AM and PM peak hours. Vehicles southbound on Mt. Hermon Road have difficulty merging onto Graham Hill Road due to congestion at PM peak commute hours caused by the narrow two-lane concrete bridge on Graham Hill Road, between Mt. Hermon Road and Highway 9. As a result, queues develop extending for some distance north from Graham Hill Road. Furthermore, traffic volumes are expected to increase in the future as employment increases in the Santa Clara Valley. Discussion during the workshop process as well as field observation indicate that this intersection is also unsafe for pedestrians, bicyclists and equestrians.

A fourth approach to this intersection will be added with the development of the Felton Commons, a public open space with a 70 space park and ride lot and a visitor service gazebo for the Felton Covered Bridge. An additional consideration is the need for buses to safely enter and exit the Felton Commons. The Transit District has requested signal preemption at this location.

This intersection, in combination with the Felton Commons, the open meadow north of Graham Hill Road, and the existing concrete bridge spanning the San Lorenzo river create an entryway to Felton and a gateway to the San Lorenzo Valley.

Signalization combined with additional traffic lanes is recommended for this intersection; in order to provide for safer and more efficient circulation and to provide for safer pedestrian and equestrian crossing. Analysis of accidents within the last 12 month period reveals that three of the four accidents reported, were those that could be prevented by a traffic signal. Since one of the goals of the Felton Town Plan is to increase pedestrian use of the Felton Covered Bridge, signalization will help provide a safer pedestrian link between the Village Core, the Covered Bridge and Felton Faire.

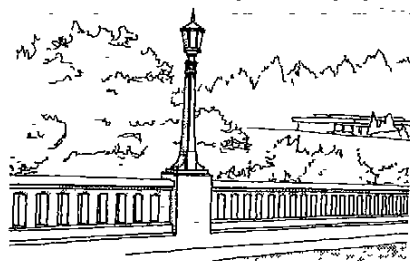
Graham Hill Road Concrete Bridge

This concrete bridge was designed by the County Engineer in 1937 to replace the Felton Covered Bridge as the eastern access to Felton from Santa Cruz. The bridge and the concrete lamps on the bridge were determined to be a favorite entryway to Felton, during the Workshop process and it is being recommended, as part of this Town Plan, that the bridge/lamp posts be added to the County Historic Resources List (see page 18)

Based upon traffic and circulation analysis, the existing two lane bridge is inadequate for existing and future traffic needs. It is recommended that four traffic lanes be provided.

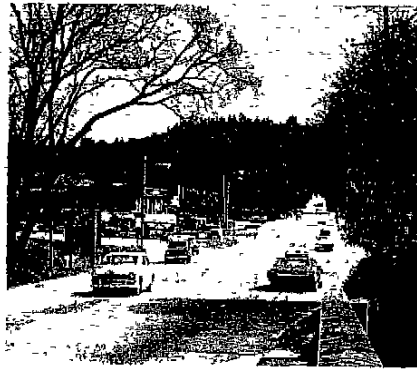
A State of California, Department of Transportation report completed in June of 1982 concluded that the existing bridge is "able to sustain not only all combinations of Legal Loads, but the State's largest Permit Load, and that seismic retrofit need not be considered".

Therefore, it is recommended that an additional two lanes be added to the south side of the existing bridge, and that the existing lanes become westbound with the two new lanes eastbound. Bicycle lanes should be provided for both eastbound and westbound traffic. Concrete lamp posts to match the original lamp posts are proposed for the additional two lanes and are proposed within a six foot wide landscaped median dividing the eastbound and westbound traffic lanes.



Graham Hill Road Bridge with Lamp Posts (circa 1937) are recommended additions to the County Historic Resources List.

Graham Hill Road Access east and west of the bridge.



A raised, landscaped median should be an integral part of the design and road geometry from the four lane Graham Hill Road Bridge east to the intersection at Mount Hermon Road. It is recommended that access not be allowed from Graham Hill Road to the meadow to the north. Vehicle access would be located too close to the intersection and/or too close to the bridge abutment. (See the plan on page 20)

A raised, landscape median should also be located on Graham Hill Road from the four lane concrete bridge, west to the intersection at Highway 9.

Parking

The consensus reached during the public workshops was that diagonal parking should replace the existing parking arrangement in which vehicles park at a 90 degree angle to the curb. Public opinion on this matter coincides with professional appraisal of parking which finds that parking angled less than 90 degrees provides for safer and easier vehicle ingress and egress, due to improved visibility while backing out, and reduced conflict with vehicles in the travelled way.

An examination of vehicle accident records for Highway 9 south of the Graham Hill Road intersection and north of Kirby Street (from the past 3 years), reveals that 60% of the reported accidents involved vehicles backing out from parking spaces. One of the reported accidents

involved a vehicle crossing the striped median to enter a parking space on the opposite side of Highway 9.

Lack of parking on Highway 9 in the village core was mentioned frequently during the workshop process. An analysis of parking demand for existing buildings in the Village area (based upon required parking per square foot as determined in the Santa Cruz County Zoning Ordinance) indicates a shortage of approximately 230 parking spaces. However, this figure can be misleading, since 110 of the 230 space shortfall would be required parking for Felton Community Hall. Currently, Community Hall has approximately 10 on-site parking spaces. However, the peak parking demand for the Community Hall occurs during times when demand for retail and business parking in the Village is minimal (i.e., during the evenings).

The highest demand for parking spaces in the Felton Village core (according to an analysis of County Zoning Ordinance Requirements), would be first for retail uses and secondly for restaurants; with retail uses needing approximately 3.5 times the parking spaces currently needed for restaurant uses. According to the County Zoning Ordinance, 396 parking spaces are required for existing retail uses in the Felton Village Core, and 116 parking spaces are required for existing restaurant uses.

Very few businesses located in the Felton Village Core provide the parking required by the current County Zoning Ordinance. And instead, depend upon the parking on Highway 9, with Roy's Market parking lot providing additional "public" parking.

The area with the greatest shortage of parking is the northern edge of the Village Core on the west side of Highway 9, just south of the Felton Empire Grade.

However, an analysis of the current use of parking areas in the village area, has not indicated a parking shortage. A survey of the on-street parking spaces on the west side of Highway 9, from Kirby Street to Graham Hill road, indicated parking at 31% capacity (weekdays at noon) to 64% capacity (weekdays at noon and Friday at 4PM). A similar survey of the parking area between Roy's Market and Heavenly Cafe indicated an average parking usage of between 48-59% of existing parking spaces. This survey was conducted during the lunch hour on weekdays, and from 4-5PM on weekdays.

It appears then, that while the Village Core may have a shortfall of parking spaces when

Continued on following page

Circulation/Parking (Cont'd)

Parking (Continued)

analyzed according to the current County Zoning Ordinance, that the parking problems stressed during the Workshop process are not issues throughout the Village Core, but are focussed in the northern Village Core area on the west side of Highway 9, and are related to specific periods of use.

Solutions to parking constraints in the Felton Village Core include:

- Expansion of "rear" lot parking, coordinated with rear access to businesses, and the creation of pedestrian walkways connecting rear lot parking to sidewalks on Highway 9.
- Provision of bicycle parking in the Village Core.
- Cooperative, private development of shared rear parking areas to meet the parking needs of adjacent properties and focus commercial use. (See pages 33 and 34 for examples of proposed cooperative parking arrangements.)
- Encouraging pedestrian use of the Village Core through the implementation of pedestrian amenities such as: wider sidewalks, benches, public art and history displays, lighting and bus service to the Village Core. (See discussion of Village bus service below).
- Special requirements for the location of businesses with high employee and customer parking demand to be located in areas in the Village Core where more adequate parking is available.

Continuous Turning Lane on Highway 9

The southbound and northbound travel lanes on Highway 9 in the Village Core are currently separated by a painted median line. With the existing traffic volumes, accident records and proposed change in on-street parking configuration, it is important that vehicles have a safe travel lane for accessing angled parking on the opposite side of Highway 9.

Therefore, it is recommended that as both a safety measure and a visual improvement, that a continuous left turn lane of textured pavers be provided on Highway 9 from Graham Hill Road to Hihn Street. Furthermore, at each intersection and crosswalk, raised landscaped median islands are recommended in order to provide: (a) enhanced pedestrian safety, (b) Felton "entry" sign locations, and (c) landscape opportunities.

Pedestrian Circulation

The use of the Felton Village area by pedestrians should be encouraged. Providing a pleasant "walking environment" is a key to the economic and social success of the Felton Town Plan. The "slow pace of life" and "face to face ambience" in Felton were considered by Workshop participants to be a key ingredient in their perception of Felton.

Out of town visitors should be encouraged to enjoy the walking environment in Felton. Strategically located parking, signage, historic displays and marketing events should draw visitors from their cars to the Felton Village Core. The following pedestrian improvements are recommended:

- Widening of sidewalks in the Village Core, coordinated with pedestrian amenities such as benches, landscaping, public art, historic displays, night lighting, properly scaled signage and various textures of paving
- Sidewalks should be provided on both sides of the widened Graham Hill Road Bridge and continue along the north edge of Graham Hill Road to the Felton Faire Shopping Center, and on the south side of Graham Hill Road past the Felton Commons. (See Graham Hill Road Concept Plan on page 20).
- Visitors should be encouraged to park in the Village Core or at the Felton Commons, and to experience the Felton Covered Bridge by walking to the Bridge. (See Felton Covered Bridge Policies on page 19).
- Crosswalks in the Village Core should be redesigned and relocated to conform to actual pedestrian patterns as diagrammed on page 10, Pedestrian Mapping.

Bicycle Circulation

Bicycles are a vital element of the circulation system. They are used for travel to and from work, school and shopping and other trips to specific activity areas.

The provision of safe, well signed, bicycle routes in conjunction with the provision of bicycle storage facilities in the village core can also reduce the demand for automobile parking.

Furthermore, Felton's mild climate, visitor attractions and designated scenic corridors make visitor oriented bicycle travel and bicycle rentals an attractive possibility.

Recommendations to enhance bicycle circulation in the Felton area include:

- Provide bicycle parking at the Felton Commons and in the village core, especially at locations which have been described as *favorite places*

- Provide bicycle lanes on Graham Hill Road, including the four lane Graham Hill Road concrete bridge, where feasible,
- Provide bicycle lanes on designated scenic corridors, where feasible.

SCMTD Circulation Route

One of the main features of the Felton Town Plan, is to once again focus activity in the "downtown" Felton Village Core. Currently, Santa Cruz Metropolitan Transit District (SCMTD) bus routes avoid the Village Core and utilize the Felton Faire Shopping Center as a transfer point.

With the improvements Cal Trans is proposing for Graham Hill Road and for the Graham Hill Road/Highway 9 intersection, the existing east-bound bus stop on the northern edge of Foster's Freeze and the northbound bus stop on Highway 9 north of the intersection will most likely need to be relocated.

Although a new bus stop is proposed, in this plan, for the Felton Commons, this bus stop is too far east to usefully serve the Felton Village Core.

In keeping with the policy of increasing activity in the Village Core Community Commercial District, it is recommended that the Transit District initiate a **Felton Village Core loop bus service** with bus stops on Gushee Street and Highway 9 as well as the Felton Commons and Felton Faire Shopping Center. (See the Circulation Improvements Map on page 23.)

Roaring Camp Connections

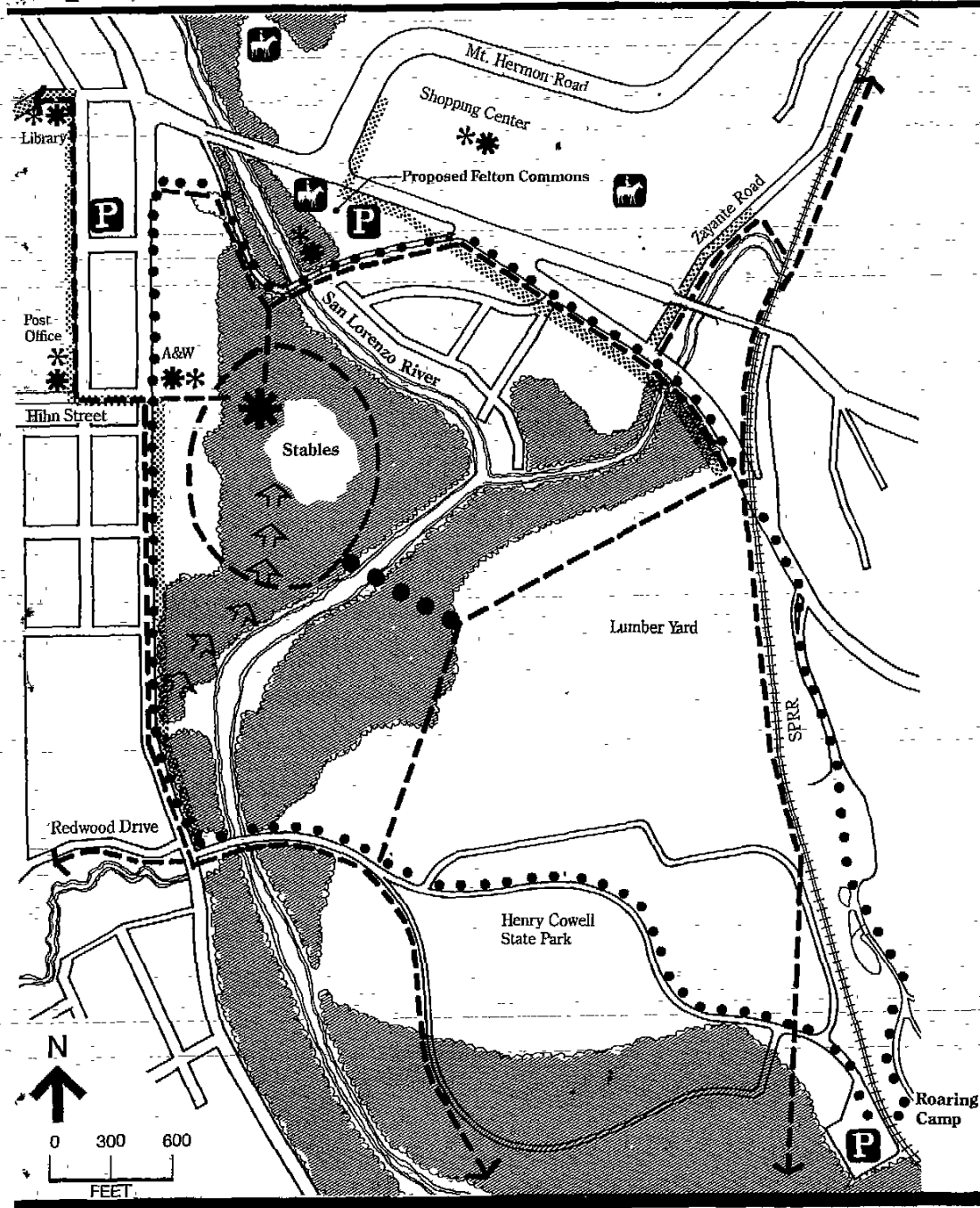
Strategies for bringing Roaring Camp visitors to the Felton Village area should be explored. A rubber-wheeled trolley connecting Roaring Camp to Felton Village via Henry Cowell State Park is one possibility.

Another possible connection to Roaring Camp would be a horse-drawn wagon which looped from Roaring Camp over the Felton Covered Bridge and then to the Village Core. Road improvements, a bridge over Zayante Creek and changing the entry ramp slope on the Felton Covered Bridge might be required for this solution. Either scheme could be privately funded and have a fee charged—or could be a free "loss-leader" sponsored by Village businesses to bring out of town visitors into the Felton Village Core.

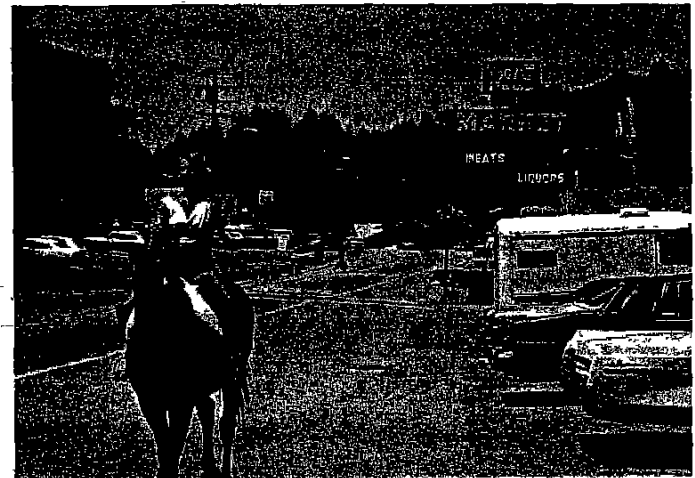
Summary of Recommended Circulation Improvements

- The County should work with Cal Trans, SCMTD and other agencies as required to coordinate and implement the circulation proposals in this Town Plan, which include:
- Signalization of the Graham Hill Road/Mount Hermon Road intersection with signal preemption for SCMTD buses,
- The addition of two lanes to the existing Graham Hill Road concrete bridge. The existing two lanes should become the entry to Felton Village. Raised, landscape medians should be provided from Mount Hermon Road to the Highway 9 intersection,
- The intersection of Highway 9 and Graham Hill Road should be re-designed to accommodate two southbound left turn lanes from Highway 9 to Graham Hill Road eastbound,
- Parking stalls on Highway 9, from Graham Hill Road to Hihn Street should be changed from the existing perpendicular parking arrangement to angled parking. The angled parking should be coordinated with the following additional improvements: 1) widening of existing sidewalks on Highway 9, wherever feasible, and 2) installation of a continuous left turn lane of textured pavers with raised landscaped "entry" islands.
- Solutions to existing parking constraints, including: 1) expansion and re-design of rear lot parking, and 2) cooperative, private development of shared rear parking areas, should be encouraged,
- SCMTD should initiate a Felton Village Core circulation loop with transit stops at strategic locations identified in this report ("Favorite Places" for instance),
- Pedestrian improvements should be provided which will encourage people to walk "downtown" and which would provide a safer, more pleasant atmosphere. Crosswalks should be constructed of interlocking paving stones,
- Innovative "transit" connections from the Felton Village Core to Roaring Camp should be explored. For example, one idea discussed at the Felton Town Plan workshops was the possibility of horse-drawn wagons or rubber-wheeled trolleys connecting the Village Core with Roaring Camp via Henry Cowell State Park.

Equestrian Circulation

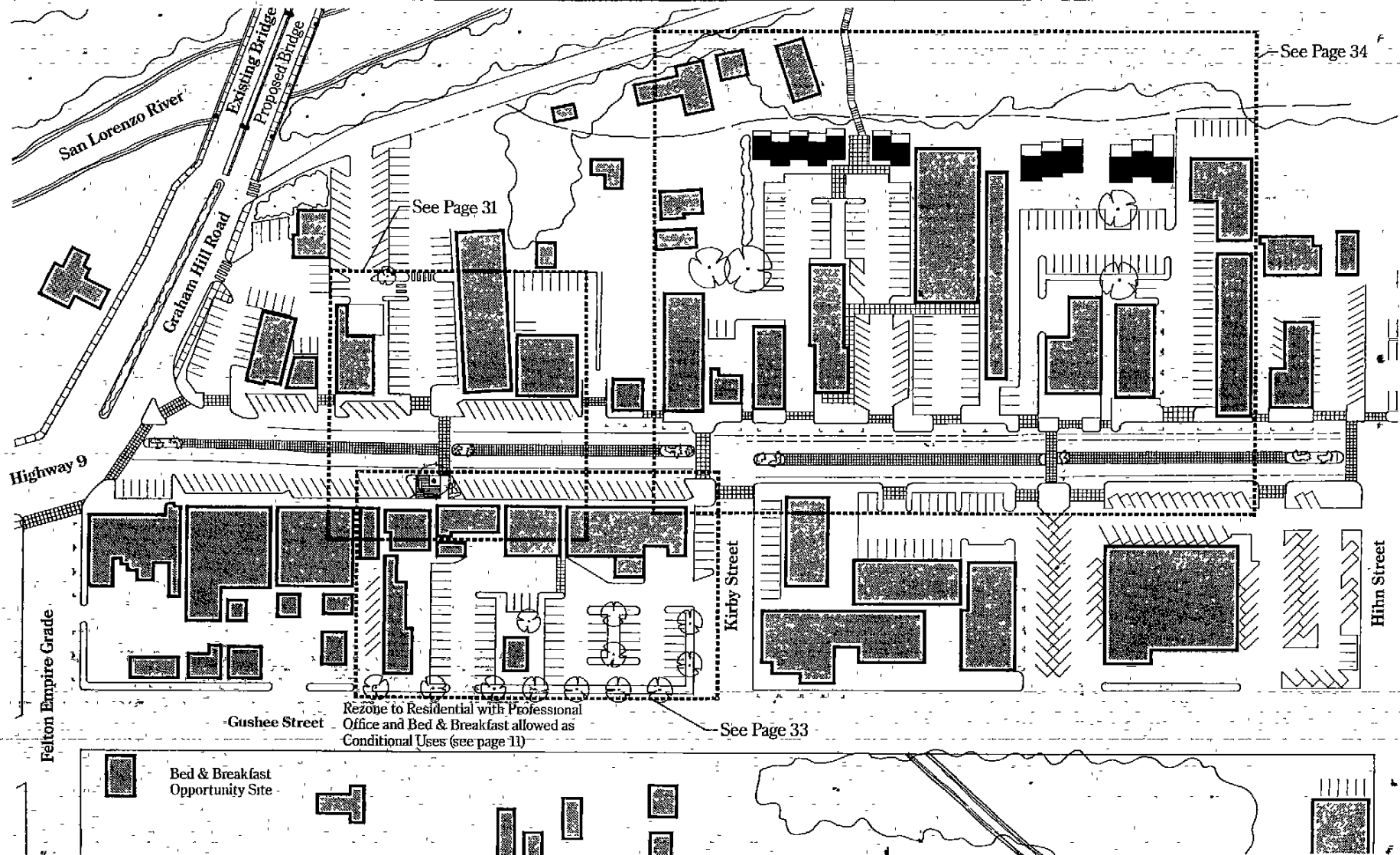
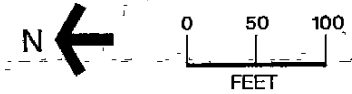


- Horse drawn wagon or trolley route
- New or improved equestrian trails.
- * Equestrian watering troughs
- * Equestrian hitching posts
- Five foot (5') shoulder for equestrian use:
 1. on the south side of Empire Grade to Fall Creek Park
 2. on the south side of Graham Hill Road
 3. on the east side of Zayante Road
 4. on the east side of Highway 9 from Graham Hill Road to the entry of Henry Cowell State Park
 5. on the south side of Hihn Street from Highway 9 to Gushee Street
 6. on the west side of Gushee Street from Hihn Street to Felton Empire Grade.
- ◀◀◀◀◀ Investigate alternate access to the existing equestrian stables, from Henry Cowell State Park
- 🐎 Where feasible, incorporate equestrian pathways and amenities into future development of public and private property now accessible to equestrians.
- P Proposed visitor serving parking areas.
- Develop an equestrian crossing at the San Lorenzo River.

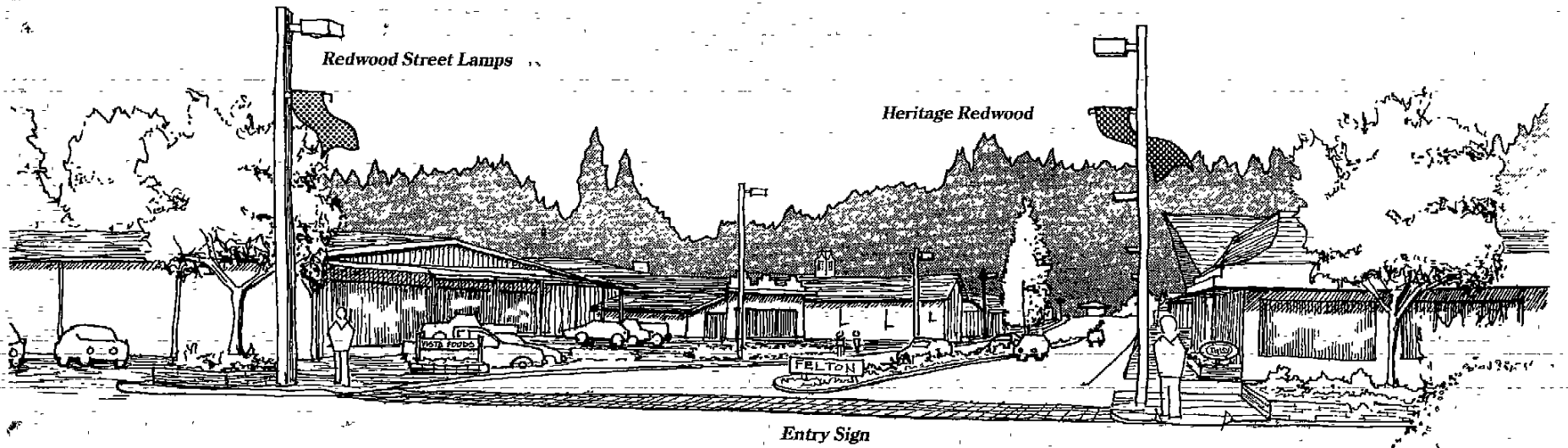
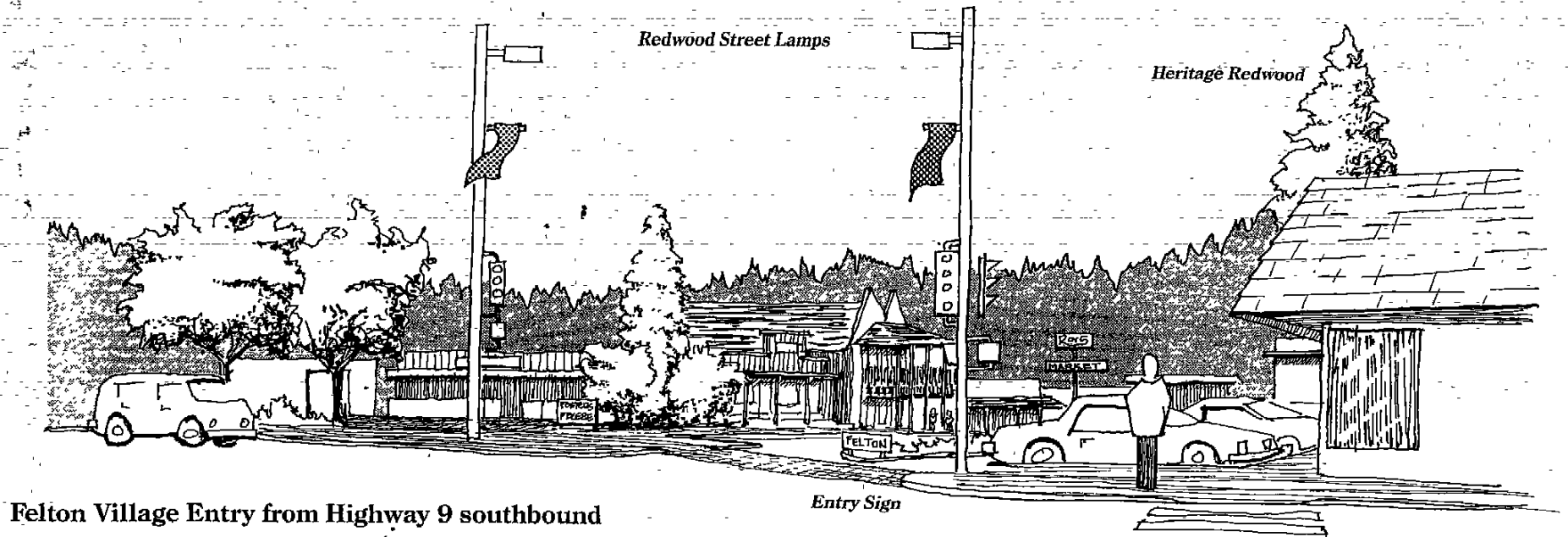


Village Design Concept Plan

The Village Design Concept Plan illustrates the design recommendations proposed for Highway 9 and the Felton Village Core. Larger scaled plans are illustrated on the following pages, and include: (1) Highway 9 Design Plan on pages 29-30, (2) A Pedestrian Focal Point Plan for the Roy's Market vicinity on pages 31-32, (3) Parking Opportunities on private property at the intersection of Gushee Street and Kirby Street on page 33, and (4) Infill and Parking Opportunities on private property east of Highway 9 on page 34. An Existing Conditions Plan of the Felton Village Core is illustrated on page 9.



Village Entry Sketches



Highway 9 Design Plan

The Highway 9 Design Plan illustrated on these two pages has been developed from the ideas generated during the public workshop process and through a continued dialogue with the Felton Business Association and Cal Trans, the State agency responsible for design and engineering efforts on State Highway 9.

The Highway 9 Design Plan utilizes the existing Highway 9 paved street section and essentially develops a continuous left turn lane for improved access to local businesses on Highway 9. Raised, landscaped median-islands are provided at each intersection and pedestrian crosswalk. Crosswalks and the continuous left turn lane are designed with textured pavers which provide both an improved level of public safety and an enhanced visual appearance for the Felton Village Core.

There are several significant features of the Highway 9 Design Plan:

1. improved vehicular access to local businesses through the provision of a *designated* left turn lane,
2. enhanced pedestrian safety and amenities through the development of cross-

walks of textured pavers, pedestrian islands at crosswalks, continuous side-walks, and increased landscape opportunities,

3. conversion of the existing perpendicular parking on State Highway 9 to 60 degree, angled parking as a safety feature,
4. the addition of raised, landscaped entry islands with Felton "entry" signs, and low shrubbery and planting as an aesthetic and safety feature,
5. the preservation and protection of the existing heritage redwood tree and the design of a wood deck and seating adjacent to the tree (see page 31).

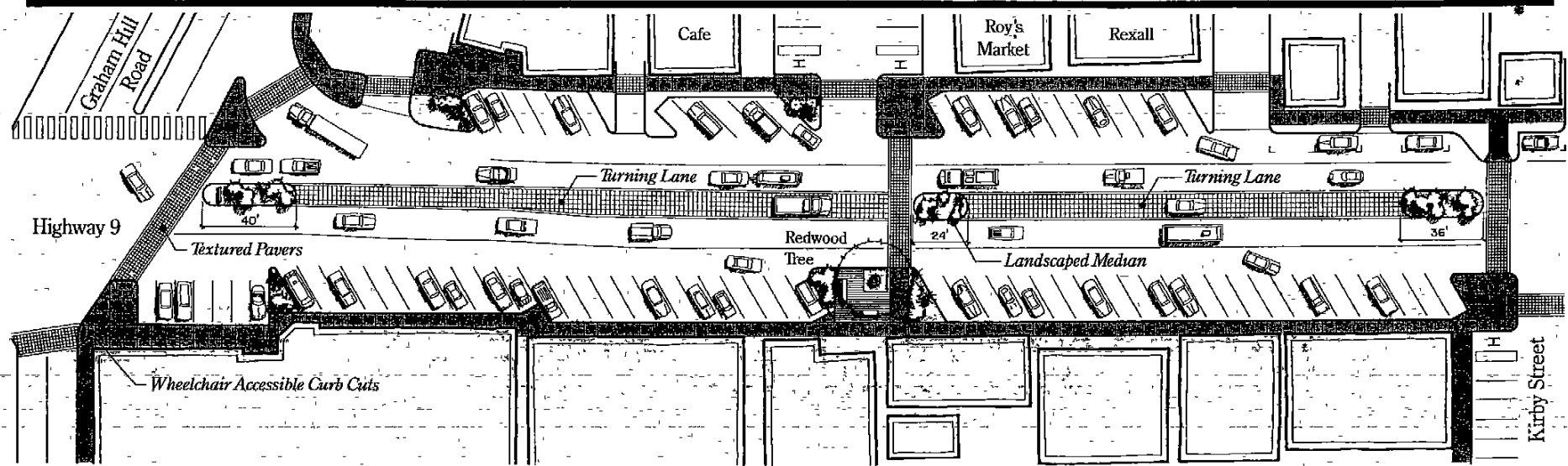
Initially, during the public workshop process, continuous raised landscaped medians were proposed for Highway 9 (see page 6, Workshop #3). However, merchants were concerned that this scheme would inhibit vehicular access to many local businesses. Therefore, several meetings were held in addition to the five public workshops for a continued dialogue between local merchants, Cal Trans representatives and the County Planning Department. During these

meetings, it was agreed to modify the continuous medians originally proposed to the continuous left turn lane with raised, landscaped islands at each intersection and crosswalk location.

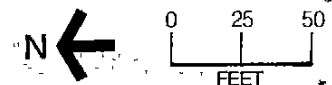
Additionally, consideration has been given to maintaining the seven existing perpendicular parking spaces on the northwest section of Highway 9, in order to ensure continued customer parking for businesses in this location, which is the area in the Village Core with the most severe parking shortfall (see page 24, Parking). The exact configuration of these parking spaces will be determined during the Highway 9/Graham Hill Road intersection design-engineering process now underway and will incorporate the continued participation of the Felton Business Association, Cal Trans and the County Planning Department.

While both blocks of Highway 9 proposed for improvements will receive the continuous left turn lanes with raised, landscaped entry islands, there are differences between the schemes proposed for each block. The north block of Highway 9 (from Kirby Street to Graham Hill Road) has a wider paved cross-

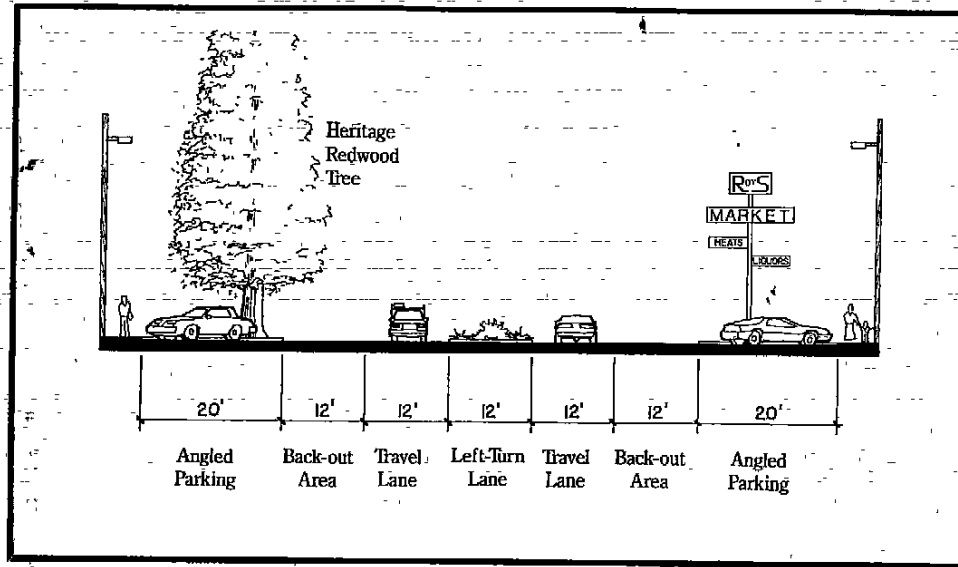
section with angled parking proposed. This configuration requires twelve feet for vehicles to back out before the twelve-foot travel lane. However, bicycle lanes cannot be provided on this section of Highway 9. Cal Trans requires bicycle lanes to be "outside" of the required back-up space for angled parking and there is not adequate street section for two five-foot bicycle lanes. The south section of Highway 9 (from Kirby Street to Hihn Street) is narrower in section than the northern block of Highway 9. Parallel parking is provided on the east side of the street and bicycle lanes are provided on both sides of the street.



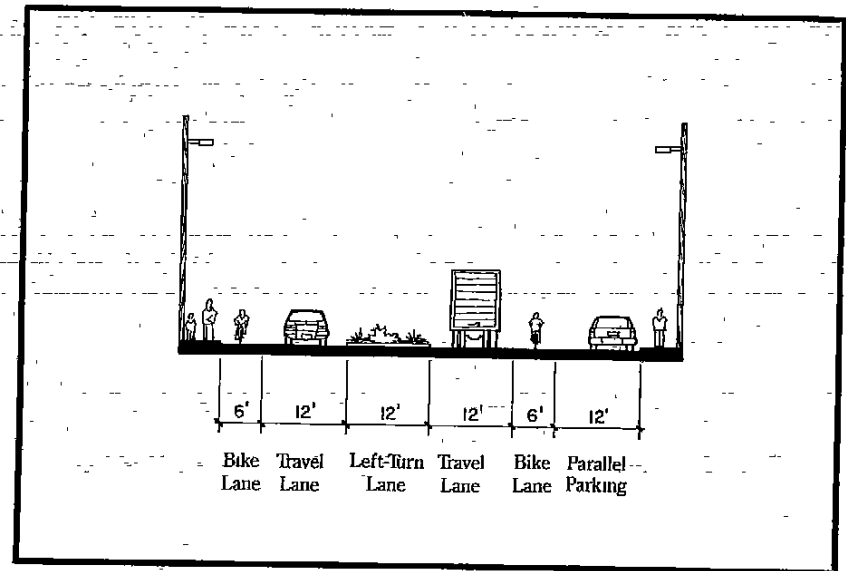
Highway 9 Improvement Plan (Graham Hill Road to Kirby Street)



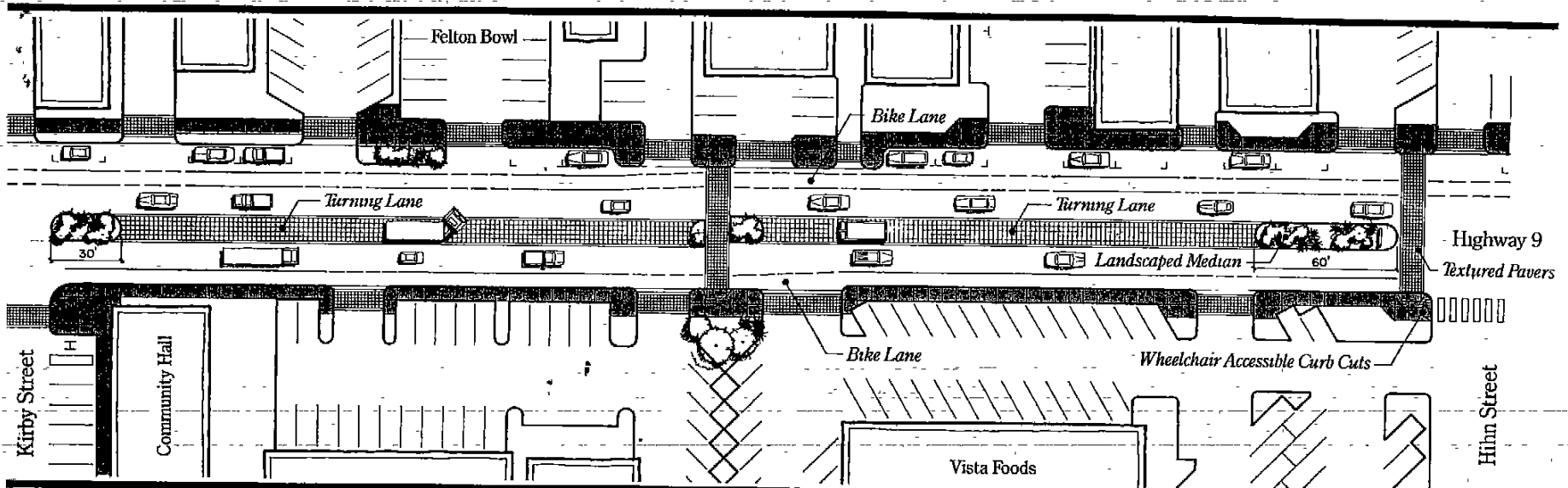
Highway 9 Design Plan



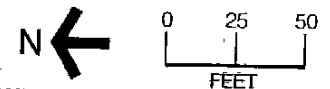
Highway 9 Street Section (Graham Hill Road to Kirby Street)



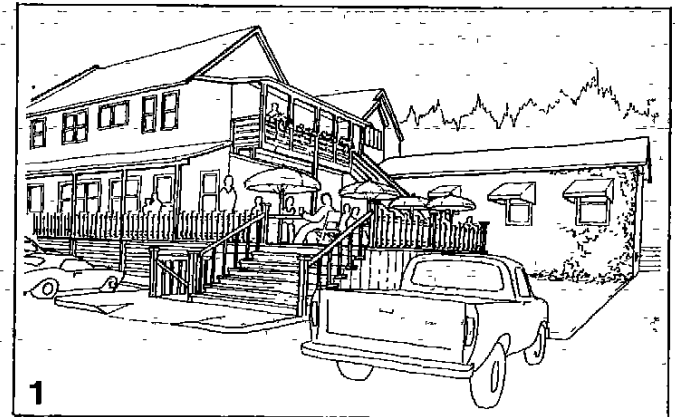
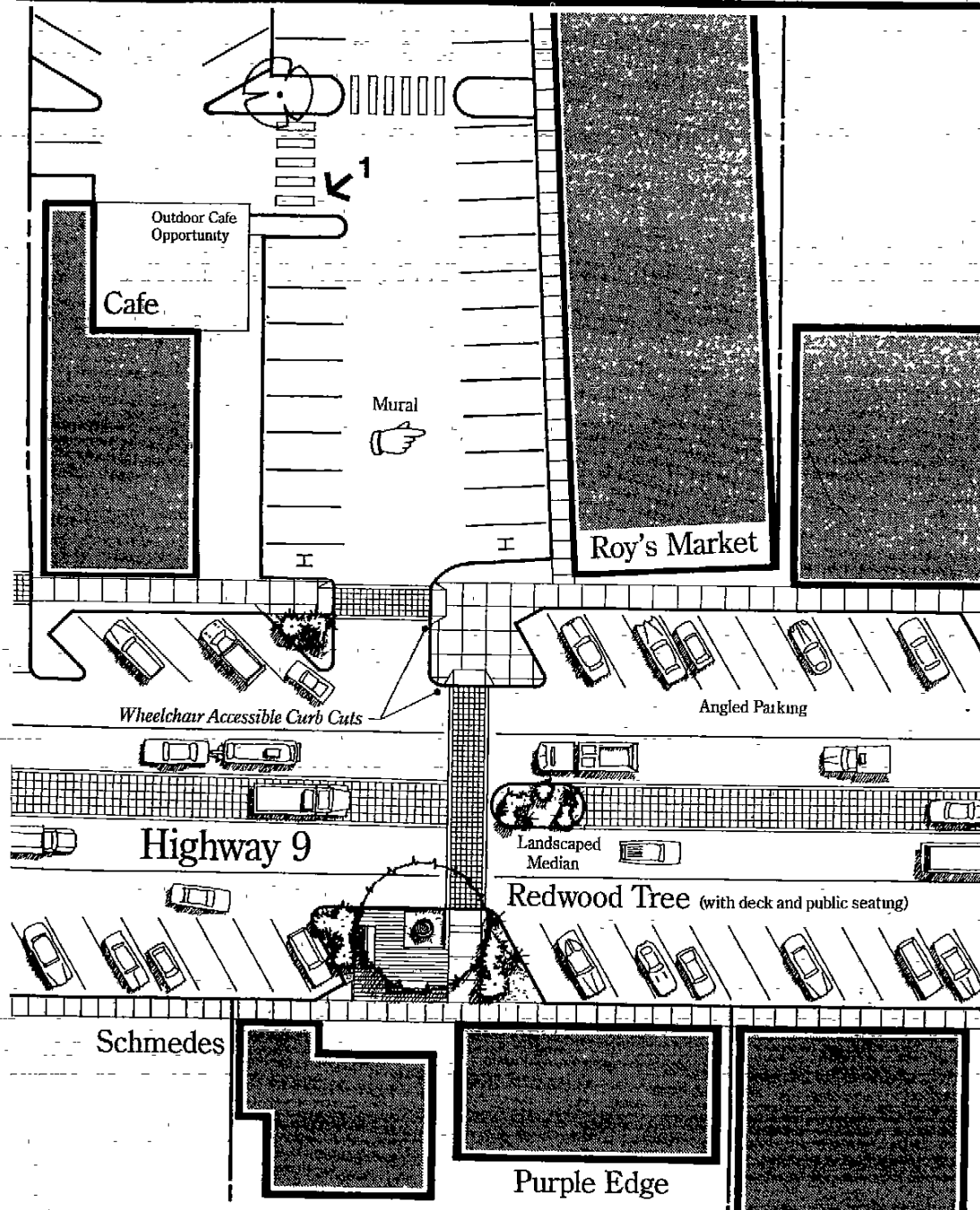
Highway 9 Street Section (Kirby Street to Hihn Street)



Highway 9 Improvement Plan (Kirby Street to Hihn Street)



Pedestrian Focal Point



The parking area serving Roy's Market and the Heavenly Cafe is a focal point of pedestrian activity and circulation. An outdoor cafe takes advantage of this pedestrian traffic and attracts Felton Bridge visitors to the Felton Village area.

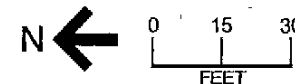
The area illustrated on the plan drawing to the left is the primary vehicular and pedestrian focal point of the Felton Village Core. There is an opportunity of creating a strong pedestrian link from the Felton Covered Bridge to the Village Core, by enhancing the existing walking environment and by providing visitor parking for the Covered Bridge in the Village Core area.

This would attract out of town visitors *already in Felton* to visit the Covered Bridge to explore the Felton Village area.

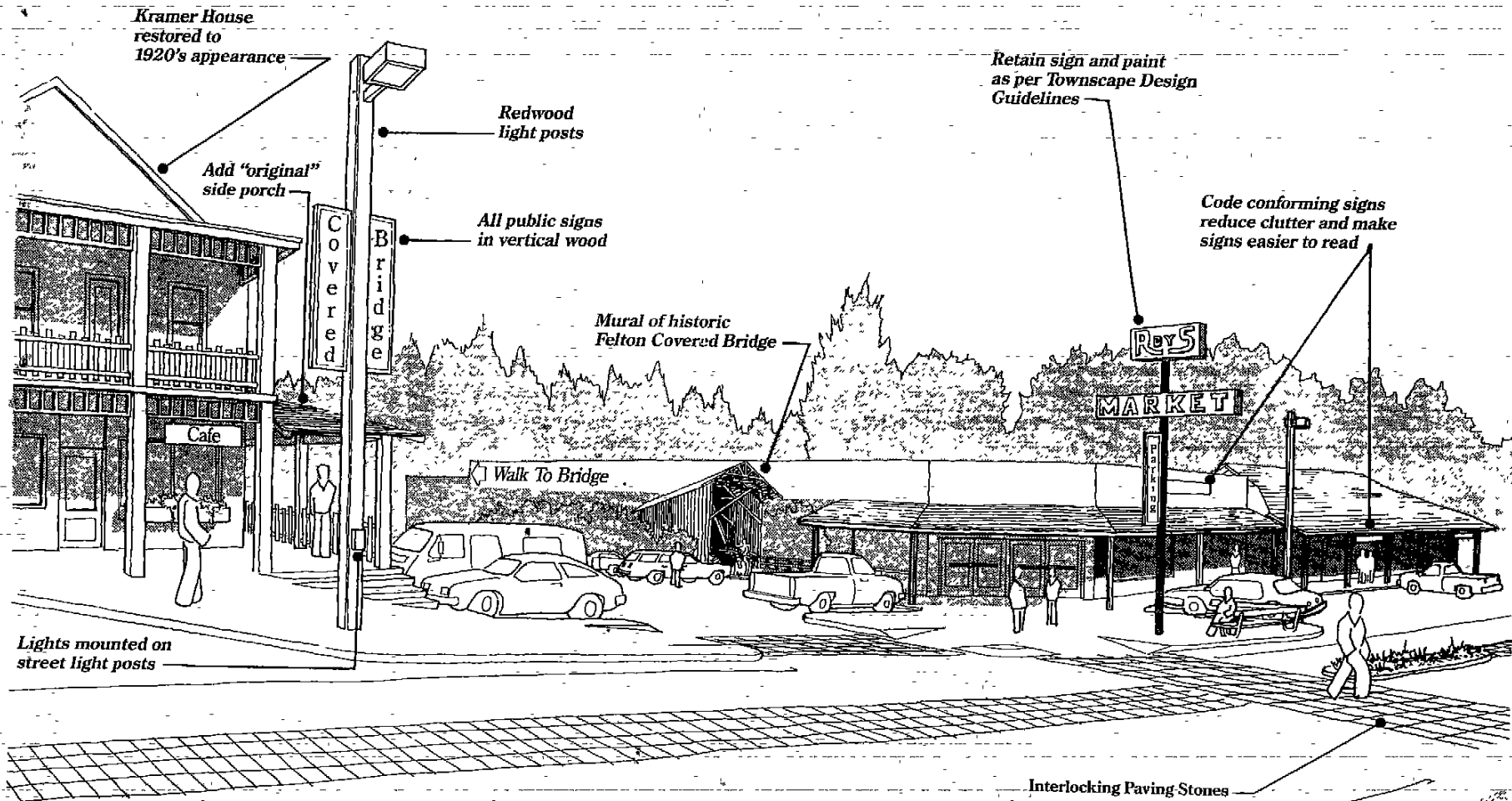
Incremental improvements to the existing architectural fabric are illustrated which would create a more pleasant and exciting walking environment, including:

- a mural of the Felton Covered Bridge
- providing visitor parking for the Covered Bridge in the Roy's Market parking area
- adding an outdoor cafe to Heavenly Cafe
- restoring the Heavenly Cafe building to a facsimile of its appearance in the 1920's (including the original side porch shown above)
- retaining the Roy's Market street sign (built in the 1950's) which is a prominent and identifiable feature of the Felton Village townscape
- a textured crosswalk leading across Highway 9 to seating under the heritage redwood tree.

Note: See the section on Highway 9 Design and Circulation Criteria on Page 35.

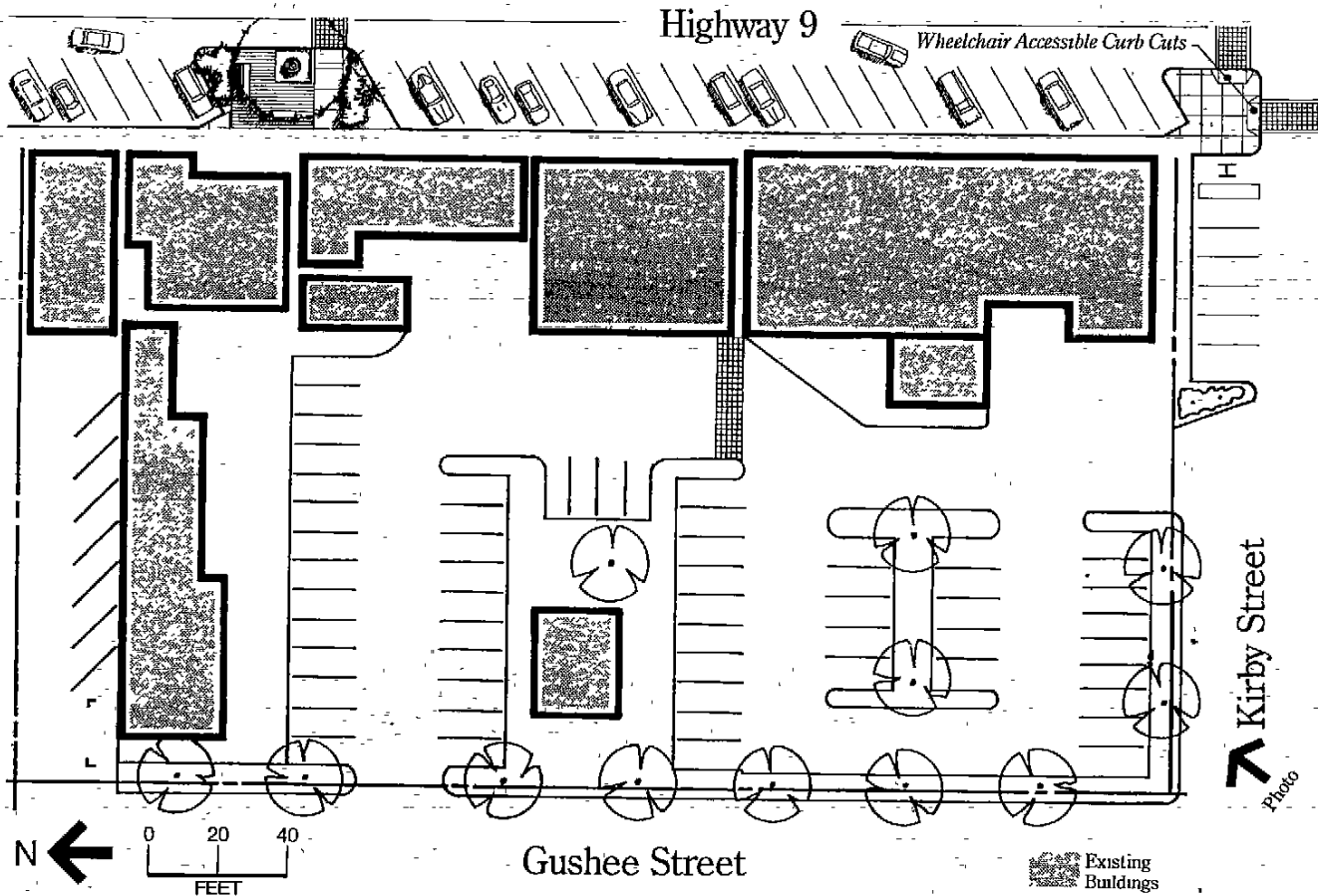


Focal Point Sketch



View east from Highway 9 towards Roy's Market

Parking Opportunities



Infill Parking Opportunity

One option for increasing the parking supply in the Village Core, is to utilize the existing privately owned parcel located at Kirby Street and Gushee Street, as illustrated on the plan above, with the following guidelines:

- provide signage on Highway 9 indicating the availability of parking at this location,
- minimize driveway access to this parking area to one driveway on Kirby Street and one on Gushee Street,
- provide a landscape buffer on Gushee Street by planting a row of deciduous trees,
- businesses should be encouraged to provide accessible "rear" doors to their shops for the convenience of customers and employees parking in this area,
- this location could also provide parking for the Gushee Street Farmer's Market. Businesses could develop concurrent sales and take advantage of the Farmer's Market traffic.



↑ Photograph of Existing Site

Planned Development

The plan on the adjacent page is a conceptual drawing of a planned development illustrating infill opportunities on the east side of Highway 9. The plan is envisioned as a guide for merchants and property owners interested in optimizing the design and development potential of their businesses and property.

The plan essentially combines private parcels and develops a shared parking plan and more efficient vehicle access to and from Highway 9. Small, live-in offices of approximately 400 square feet per floor are shown overlooking the ridge above the river valley.

This concept plan was generated in response to goals established during the public workshop process, including:

- utilizing available land in the Village Core for new development
- providing "infill" housing opportunities in the Village Core
- providing new development which overlooks the San Lorenzo River and the Felton Covered Bridge
- providing new parking opportunities in back of existing Highway 9 businesses.

A planned development such as the one illustrated here, offers benefits for both the private and public sectors, including:

- a reduction in parking requirements due to shared parking in a planned development
- more efficient utilization of land area for parking and vehicular circulation
- reduction in curb cuts for access to and from Highway 9 results in:
 - a) less interruption of circulation on Highway 9
 - b) more available customer parking on Highway 9, and
 - c) less conflict between pedestrians on Highway 9 and vehicles entering or exiting private property fronting Highway 9
- addition of needed housing in the Village Core
- creation of a pleasant pedestrian link between the south end of the Village Core and the Felton Covered Bridge/Felton Commons
- creation of design opportunities on the east side of Highway 9 that soften existing buildings and create an inviting pedestrian environment (see Townscape Elevations)

See Policy on Page 29

Infill and Parking Opportunities

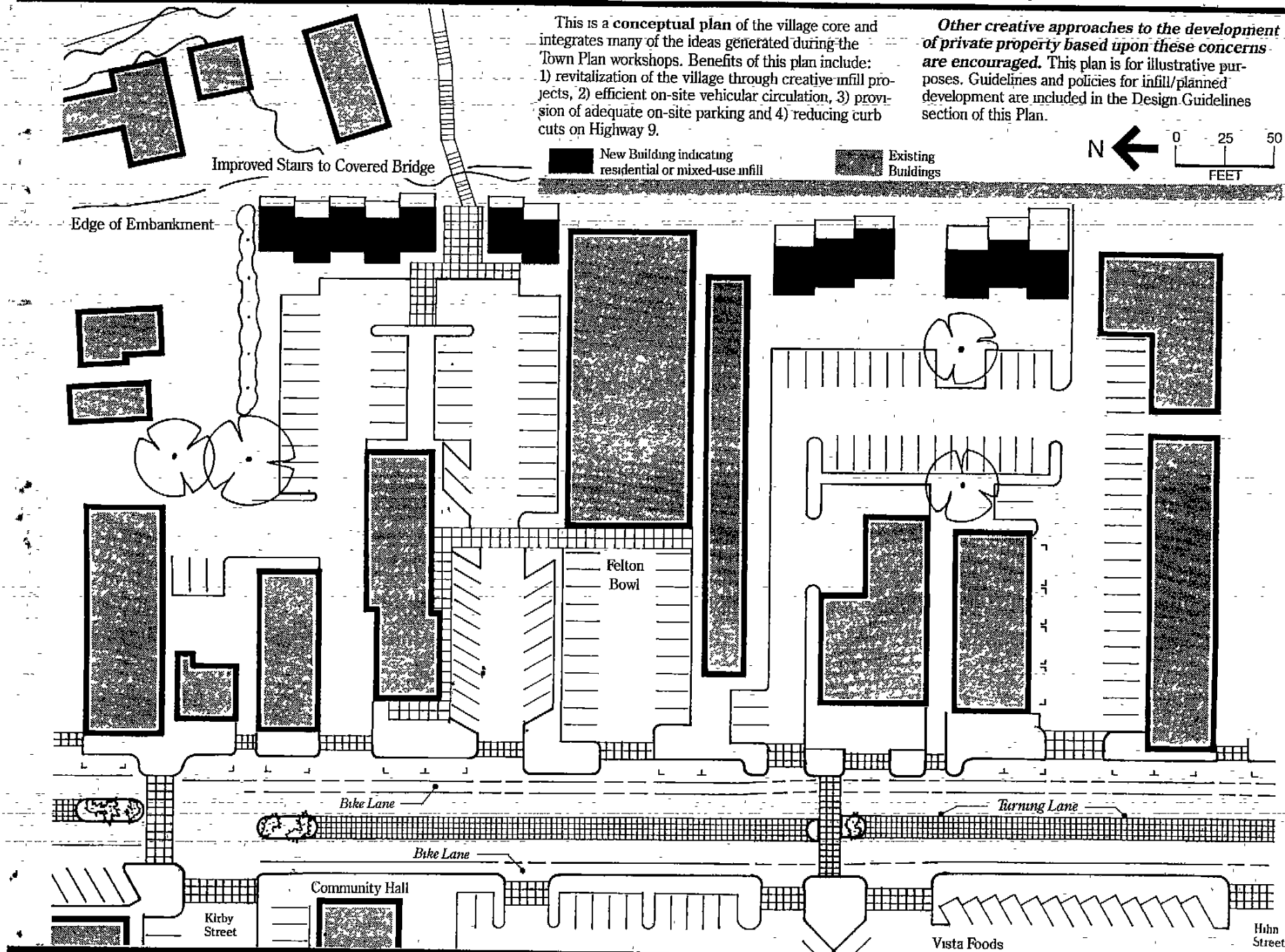
This is a **conceptual plan** of the village core and integrates many of the ideas generated during the Town Plan workshops. Benefits of this plan include: 1) revitalization of the village through creative infill projects, 2) efficient on-site vehicular circulation, 3) provision of adequate on-site parking and 4) reducing curb cuts on Highway 9.

Other creative approaches to the development of private property based upon these concerns are encouraged. This plan is for illustrative purposes. Guidelines and policies for infill/planned development are included in the Design Guidelines section of this Plan.

■ New Building indicating residential or mixed-use infill

■ Existing Buildings

N ← 0 25 50
FEET



Townscape Design Guidelines

Purpose

To develop comprehensive design guidelines which generates a harmonious, yet diverse, designed environment and encourages public and private sector revitalization of the Felton Village as the historic-center and pedestrian focal point of Felton, while re-establishing Felton's traditional role as the gateway to the San Lorenzo Valley.

Goals

- To develop an improvement program including guidelines for townscape and facade improvements, landscaping, street lighting and parking, that produce a unified visual image and enhances the relationship between Felton Village and the surrounding natural environs.
- Encourage safe and pleasant pedestrian circulation within Felton Village, separating pedestrians from vehicular circulation and parking wherever feasible.
- To maintain the "rustic", "open", "western" appearance of the Village Core as described by Workshop participants.

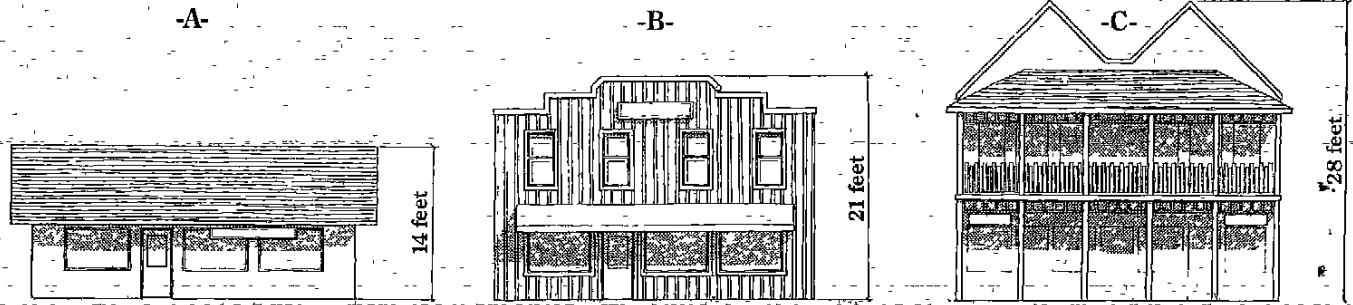
Application

These guidelines apply to all structures within the Community Commercial District (see page 11) and to all commercial, institutional and public buildings and structures within the Felton Village Study Area. In addition, the guidelines apply to all structures within the four Scenic View Corridors and to all buildings and structures directly adjacent to, or clearly visible from, roadways entering Felton (and within the Felton Study Area) which have been designated as Scenic Corridors.

Specific design guidelines for Highway 9 within the Community Commercial District apply to circulation design policies on Highway 9 and to all buildings which front Highway 9.

Highway 9 Design Criteria

- These Guidelines shall apply for all buildings fronting Highway 9 from Graham Hill Road to Hihn Street.
- New buildings, or buildings which are remodeled or restored, shall create a continuous and harmonious streetscape on Highway 9, and provide a pleasant, safe pedestrian environment.



This illustration shows a typical one story structure (-A-) converted to a two story structure with a false front facade (-B-) and in comparison to the proposed remodel of the Kramer House/Heavenly Cafe. Refer to Highway 9 Design Criteria (D), below.

- Signage and awnings, if provided, shall be placed to complement the scale and appearance of an entire building facade, rather than one business or office within a building.
- "Western" false front facades, or derivatives, are preferred.
- Existing one story, detached buildings shall be encouraged to develop full or partial second story additions. Residential use on second floors shall be encouraged, both to create a mix of uses in the Village, provide needed Village housing opportunities, and create the least impact on off-street parking requirements. See illustrations above.
- Sidewalks shall be accessible to the handicapped and disabled, and shall be widened wherever feasible to a minimum of six feet in width. All new buildings shall conform with Title 24, Part 2, Handicapped Access Regulations, as established in the California Administrative Code.
 - A combination of smooth-textured pavers, exposed small aggregate concrete, smooth concrete and other materials shall be encouraged in order to provide pedestrian scale and to emphasize storefront entries, crosswalks and entries to parking areas.
- All new buildings, additions or remodels along Highway 9 shall be of consistent setback from Highway 9 in order to create a continuous street facade. Exceptions shall be granted only when significant public, pedestrian amenities are provided such as public seating or an outdoor cafe.

Policy

Pursuant to County Code Section 13.10.510 (f), setback lines on Highway 9, within the Community Commercial District, shall be reduced in order to allow development consistent with the goal of establishing a continuous streetscape in the Felton Village Core.

- Required off-street parking shall be located to the rear of new construction where possible, and shall be designed and landscaped to harmonize with adjacent uses. Where feasible, parking facilities shall be designed to provide shared, interconnected parking with adjacent facilities.
- Parking areas shall be landscaped to provide shade and screening from adjacent streets.

Highway 9 Circulation

- Emphasize pedestrian circulation and safety through the provision of:
 - continuous sidewalk areas which link pedestrian activities,
 - well marked pedestrian crosswalks, located for maximum pedestrian safety and use,
 - adequate congregating areas, pedestrian "islands" and reduced street crossing distances, wherever feasible.
- Provide a continuous left turn lane of textured pavers in order to enhance vehicular access to businesses and to provide safe turning for vehicles.
- Provide raised, landscaped entry islands as a safety feature and to provide opportunities for Felton "entry" signs and landscaping.
- Reduce the number of driveway encroachments wherever feasible and provide for maximum interconnections amongst off-street parking areas and delivery areas. Encourage infill, planned developments in order to achieve this goal.

Policy

Future development on Highway 9 in the Felton Community Commercial District should be designed in order to reduce curb cuts on Highway 9, increase off-street vehicular circulation thru the interconnection of parking facilities thereby reducing vehicular conflicts and congestion on Highway 9.

Continued on next page

General Townscape Guidelines

A. Color of signage and building materials shall be used to complement the architectural characteristics of a building or structure.

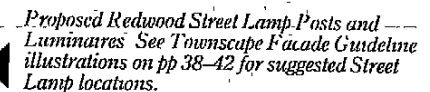
4. Natural wood with light stains or finishes are preferred. Painted wood is acceptable.

• Preference is for natural wood with color used for accents on trim elements, windows, doors, signage and awnings.

- A. All roofing materials on sloped roofs shall be on non-reflective materials and shall be of colors that complement the scenic views provided by the sloped wooded hills beyond. Acceptable roof material colors are: green, beige, tan, brown.
- B. Terra cotta, "spanish" or "mission" style roof tiles and parapet trim are not acceptable within the Community Commercial District.
- C. Parapets on false front facades shall be painted or stained in neutral, earth colors or be of natural wood or stone. Bright, primary colors which contrast with the surrounding natural environment shall not be allowed.
- D. Vertical mechanical stacks for venting of any kind shall be painted to match the roofing materials. Roof top mechanical equipment including solar and photovoltaic panels shall be screened from view. Solar panels of two or less can be mounted on visible sloped roofs if materials are non-reflective.

The height and scale of signage, light and traffic standards in the public right-of-way shall be designed to minimize impacts upon the designated Scenic View Corridors and to complement the rural character of Felton

- lighting shall be provided in adequate locations to provide for public safety,
- lighting shall be non-glare and use non-reflective materials, particularly wood where feasible,
- lighting shall be designed and located in order to complement Felton's rural appearance and to minimize scenic/visual impacts,



Landscape Design Guidelines

Landscaping and Site Planning

- A. Workshop participants expressed a desire to maintain Felton's "western" appearance and "wide open spaces", rather than developing a linear street tree program; as a result, cluster landscaping is recommended for the Felton Village. Cluster landscapes form dense landscape pockets and have the following additional benefits:
- they use the small amount of available planting area in the Felton Village Core in an efficient manner
 - the variety of plant species in cluster landscapes create a microclimate conducive to each plants' survival
 - cluster landscapes provide a variety of plant types and is thus less disease prone than monoculture or linear street tree planting, and
 - therefore maintenance and initial costs for irrigation and other plant care is minimized.
- B. Cluster landscapes should include tall, canopy trees, smaller understory trees and ground level shrubs and herbaceous plants.
- C. Trees with colorful foliage and plants which flower seasonally are encouraged.
- D. Cluster landscapes should be located adjacent to pedestrian focal points and seating areas.
- E. Indigenous species shall be given priority. Other species shall be considered upon the recommendation of a licensed Landscape Architect or certified Horticultural Consultant.
- F. Highway medians and medians proposed on Graham Hill Road should be cluster landscaped with ground level shrubs and herbaceous plants. Plant materials with seasonal leaf and flower changes are encouraged.
- G. Plants and landscape materials utilized to meet screening requirements as specified in Scenic View Corridor #3 and #4 Guidelines shall complement the existing natural plant communities and shall be specified by a licensed Landscape Architect or certified Horticultural Consultant.
- H. Proposed roads, pathways and parking areas within designated Scenic View Corridors shall be screened from public view, and shall be constructed of materials which are non-reflective and which harmonize with the surrounding natural environs.

Suggested materials for roads and parking areas located within designated Scenic View Corridors include, earth colored integral coat concrete (or equal), or asphalt with integral curb

Suggested materials for pedestrian pathways in designated Scenic View Corridors include: soil cement, asphalt, or other earth tone materials.

I. **Redwood Tree on Highway 9:** These policies are designed to protect and enhance the redwood tree located on the west side of Highway 9, between Kirby Street and Felton Empire Grade, a major focal point—and the tallest "structure"—in the Felton Village area.

- 1) An arborist or certified Horticultural Consultant should be hired to develop an ongoing maintenance program for the redwood tree, including guidelines for pruning, irrigation and feeding, and care of the root system.
- 2) One parking space on each side of the redwood tree should be removed and a redwood deck shall be constructed as illustrated in this Plan
- 3) Utility wires adjacent to the redwood tree will be removed as part of the undergrounding project (see pp 11 and 12).
- 4) Road improvements proposed in this Plan, sidewalk widening, the undergrounding of utilities and any proposed work on underground storm drain improvements shall be designed in accordance with recommendations in Item 1 above, and a certified Horticultural Consultant or Landscape Architect shall be present during any proposed construction on Highway 9 to ensure protection of the redwood tree.

Recommendation

The County should develop a Heritage Tree Inventory with policies and guidelines designed to enhance, protect and maintain significant trees.



Redwood tree on the west side of Highway 9, the tallest "structure" in Felton

Library/Post Office

Within the near future, the existing Felton Branch Library and the existing Felton Branch Post Office will need to expand their facilities. Both currently lease buildings on Gushee Street.

The Post Office is located on the south end of Gushee Street and has been identified as a *favorite place* due to its intimate scale and friendly, responsive service.

The Branch Library is located in the Faye Bilardi Memorial Building on the Northern edge of Gushee Street. The Bilardi building is listed in the *National Register of Historic Places* and has been identified as a *favorite place* in Felton due to its intimate scale, friendly service and the historic character of the Bilardi building.

The *Long Range Plan for Library Service* (1986), has identified the need for construction of a new 16,000 square foot Branch at Felton to provide Tier-II service to San Lorenzo Valley and Scotts Valley Residents."

A. Retaining both public facilities as they expand in the Felton Community Commercial District, or directly adjacent to the district, is a major priority in the continued revitalization of the Felton

Village and is consistent with General Plan policies regarding focusing community activities in Village centers.

- B. The community-serving use of the historic Faye Bilardi Memorial Building shall be maintained if the Branch Library relocates to larger facilities.
- C. Potential sites for future expanded facilities for the Library and Post Office should be explored; including the possible utilization of portions of the existing County Public Works Yard in the south Village Core and the expansion of the Post Office onto adjacent vacant property on the south end of Gushee Street.
- D. In order to retain the intimate scale of both facilities as they expand and to avoid the institutionalization often inherent in large public facilities, the following guidelines shall apply to the expansion of either facility:
1. new facilities shall be designed as a cluster of small component structures, rather than a monolithic building.
 2. new facilities shall conform to the Felton Town Plan Design Guidelines.
 3. plans for either new or expanded facilities shall incorporate a community participation process to ensure the visual and functional compatibility of these facilities with the Felton community.

Gushee Street Buffer Zone

- A. A buffer zone on the east side of Gushee Street shall be created in order to screen commercial activities from the residential uses on the west side of Gushee Street.
- B. New and existing refuse containers, outdoor storage, mechanical equipment and parking areas allowed under current County Ordinances shall be screened from view by landscape buffers and/or wood screens or fences.
- C. Chain link fencing or metal fencing shall not be allowed.
- D. A landscape buffer utilizing a variety of native evergreen trees, deciduous trees and ground level shrubs shall be installed on the east side of Gushee Street in order to screen commercial uses and parking areas.

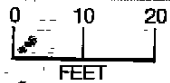
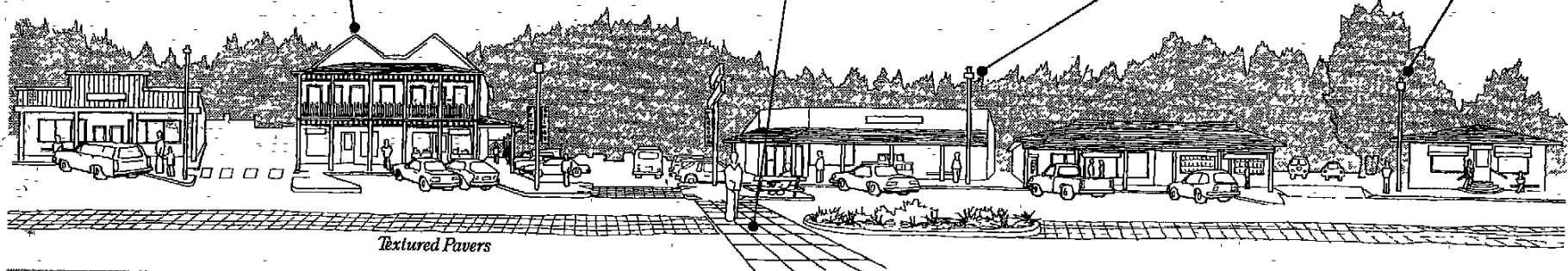
Townscape Facade Guidelines

Major Opportunity for Village Focal Point

Restore to 1920's appearance and retain twin gable false front

Crosswalk with widened "curb island" for pedestrian safety and wheelchair accessible curb cuts.

Redwood street lamps



Create a pedestrian focal point with textured crosswalks and public seating

Illustrative Townscape Elevations

Historic Kramer House (1878)

Sign out of scale with building

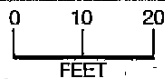
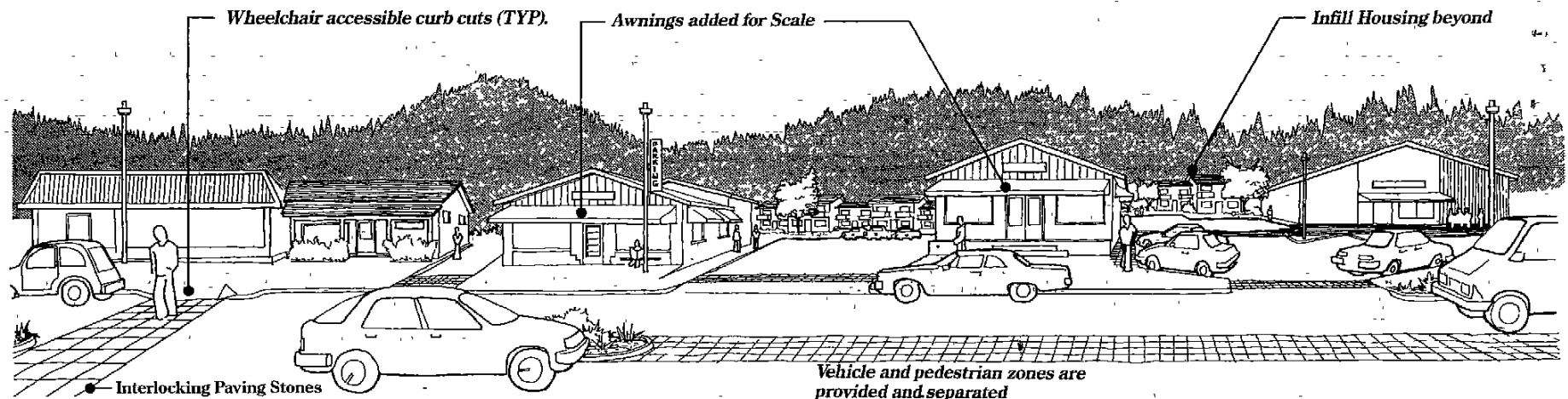


Highway 9 Eastside

Existing Townscape Elevations

Townscape Elevations illustrate the Highway 9 streetscape with building signage which meets County Sign Ordinance requirements.

Townscape Facade Guidelines



Illustrative Townscape Elevations

Continuous access for vehicles to and from Highway 9 creates unsafe circulation for vehicles and bicyclists on Highway 9

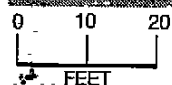
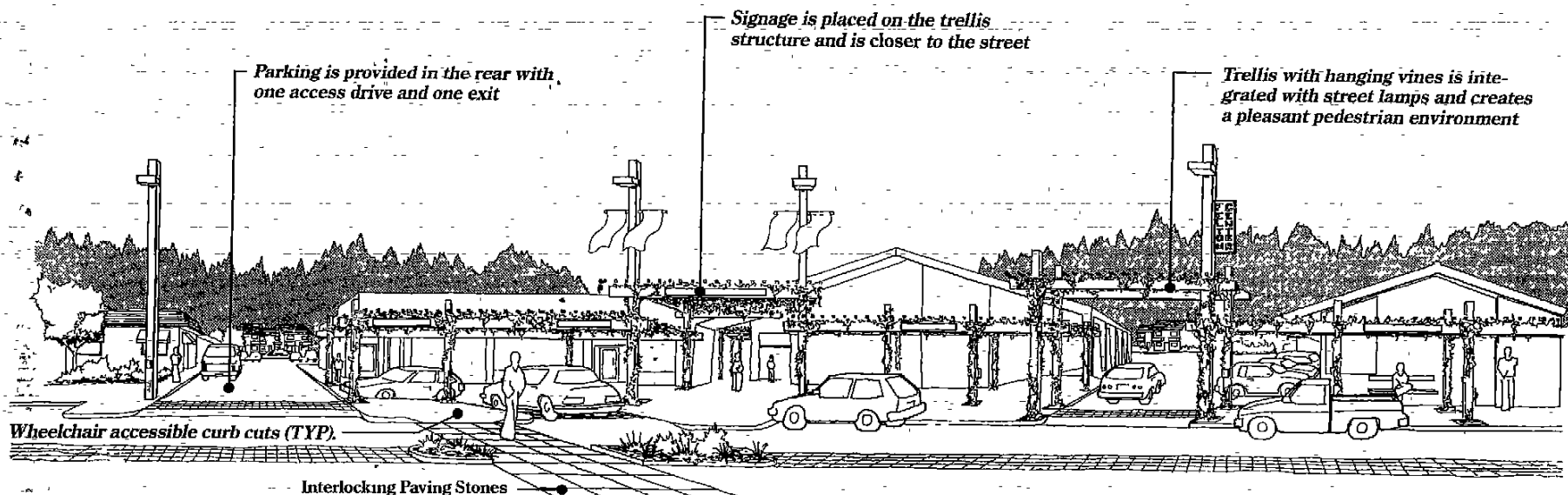


Highway 9 Eastside

Continuous paving leaves no room for safe pedestrian zones

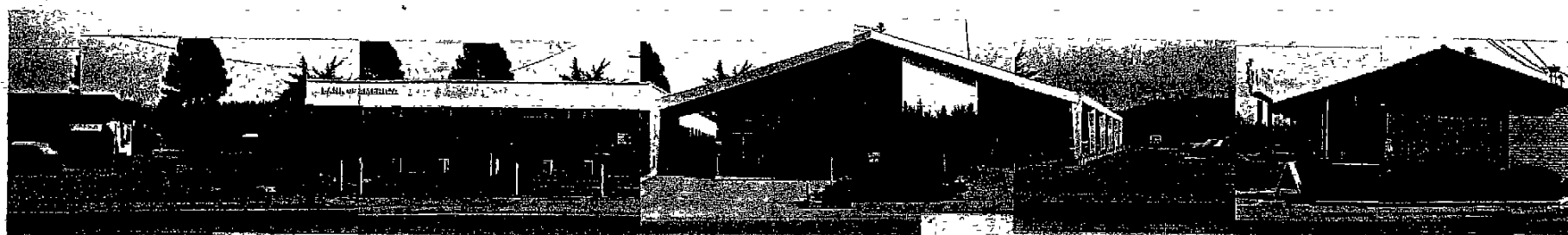
Existing Townscape Elevations

Townscape Elevations illustrate the Highway 9 streetscape with building signage which meets County Sign Ordinance requirements.



Highway 9 View of Infill Development Concept

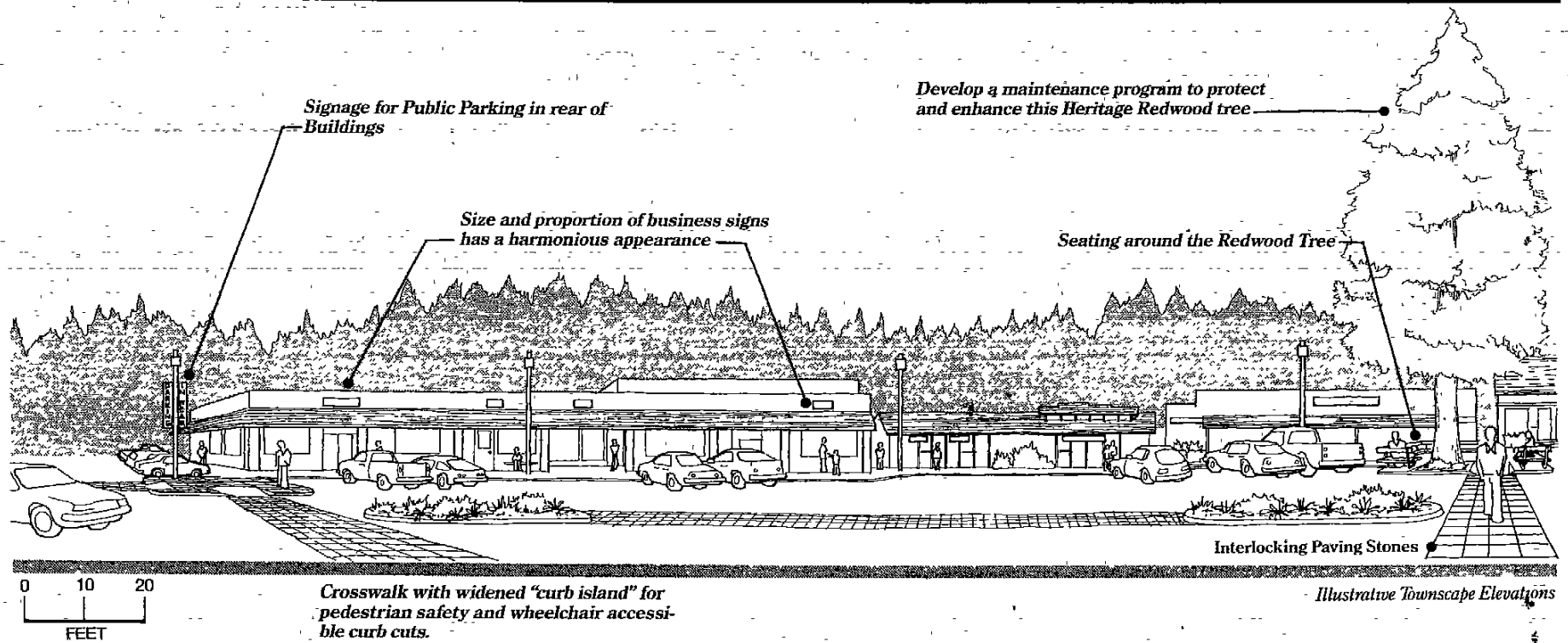
Illustrative Townscape Elevations



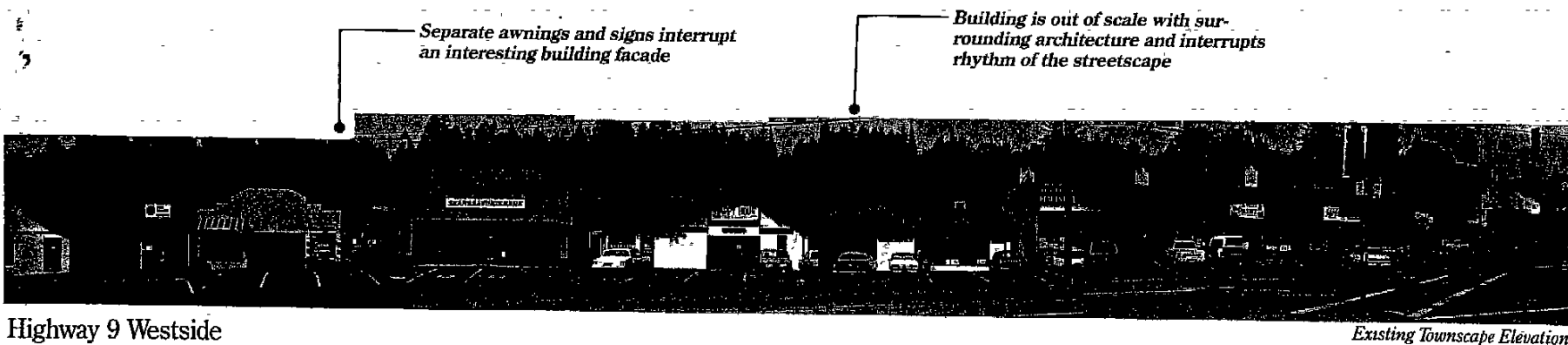
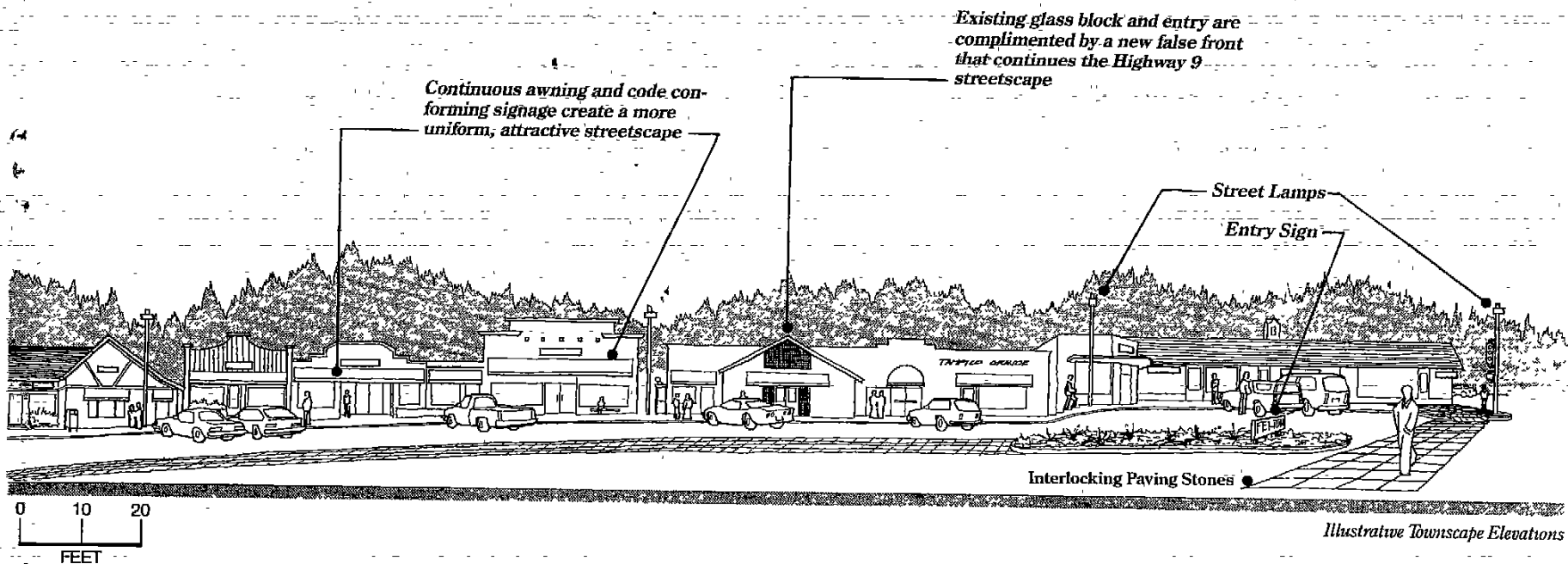
Existing Townscape Elevations

Townscape Elevations illustrate the Highway 9 streetscape with building signage which meets County Sign Ordinance requirements.

Townscape Facade Guidelines



Townscape Elevations illustrate the Highway 9 streetscape with building signage which meets County Sign Ordinance requirements.



Existing "False Front" Facades

Townscape Elevations illustrate the Highway 9 streetscape with building signage which meets County Sign Ordinance requirements.

Start-up Projects

The following improvement projects could be realized in a relatively short period of time, and installed in phases as funds or contributions become available.

The cost estimates provided are unit cost estimates based upon the preliminary and conceptual design illustrated in this Plan. **Volunteer labor, and/or the donation of materials or in-kind services would substantially reduce the estimated costs.** Records of all volunteer work, in-kind services and donated materials should be kept in order to document community commitment for future grant applications.

Potential funding sources and responsible local and state agencies are indicated in italic print after each improvement project.

	Labor + Materials
<ul style="list-style-type: none"> ■ Work with local artists to design and paint a mural of the Felton Covered Bridge on the north wall of Roy's Market. This would require a long term lease on the mural-wall granted by the building owner. <ul style="list-style-type: none"> • S.C. County Cultural Council • California Arts Council • Felton Business Association • Felton Town Association • Community Organizations • Private Donations 	\$15,000.00
<ul style="list-style-type: none"> ■ Re-stripe Roy's Market parking lot with 40% compact parking, designated employee parking and Covered Bridge visitor's designated parking, adjacent to Covered Bridge Road. Parking and access easements would have to be negotiated with the property owner. <ul style="list-style-type: none"> • Participating property owners and businesses 	\$3,000.00
<ul style="list-style-type: none"> ■ Landscape west entry area of the Felton Covered Bridge, in combination with wood bollards in order to prevent vehicle parking adjacent to the bridge and to create a more pleasant, dust-free pedestrian environment. Easement and maintenance agreements would be required between the County and adjacent property owners. <ul style="list-style-type: none"> • County Parks Department • State/Federal Grants • Felton Town Association • Felton Business Association • Community Organizations • Private Donations 	\$4,800.00
<ul style="list-style-type: none"> ■ Restripe the existing crosswalk on Highway 9, in front of Roy's Market with bold, white graphics, 24" x 96". Striping should run north/south. This is a temporary improvement until the textured crosswalk is installed with proposed Highway 9 improvements. <ul style="list-style-type: none"> • Cal Trans 	\$1,000.00
<ul style="list-style-type: none"> ■ Install redwood stairs with handrails on the existing dirt path connecting the Felton Covered Bridge with the east edge of the Felton Bowl vicinity. See drawings on pages 20 and 34. Develop access and liability agreements with adjacent property owners as required. <ul style="list-style-type: none"> • County Parks • State/Federal Grants • Felton Business Association • Community Organizations • Private Donations • Felton Town Association 	\$9,000.00
<ul style="list-style-type: none"> ■ Remove parking space on each side of heritage redwood tree located on Highway 9 and install redwood deck with planting and benches. <ul style="list-style-type: none"> • Cal Trans • County Public Works • County Parks • Felton Town Association • Felton Business Association • Community Organizations • Participating Business/Property Owners • Private Donations 	\$14,500.00
<ul style="list-style-type: none"> ■ Hire an arborist to develop a comprehensive program for protecting the Redwood Tree. This program should include guidelines for: tree pruning, watering and irrigation, and protection of the root system. <ul style="list-style-type: none"> • Santa Cruz County Cultural Council • Local Equestrian Groups • County Parks • Felton Town Association • Felton Business Association • Community Organizations • Private Donations 	\$1,800.00
<ul style="list-style-type: none"> ■ Initiate a signage program to upgrade existing Village signage to meet guidelines established in the County Sign Ordinance and in this Town Plan: <ul style="list-style-type: none"> * distribute copies of the County Sign Ordinance to all businesses in the Village area * obtain low interest loans from a local financial institution for businesses who voluntarily upgrade signage to meet existing Guidelines. * Request County to expedite permit review and/or waive permit fees for signage for upgrading for a period of 18 months from the approval of this Plan. <ul style="list-style-type: none"> • Felton Business Association • S.L.V. Chamber of Commerce • County Planning Department • Felton Town Association 	(Estimated Fee Loss) \$15,000.00
<ul style="list-style-type: none"> ■ Promotion and administration costs for a Felton Farmer's Market, to bring people downtown. <ul style="list-style-type: none"> • Felton Town Association • Felton Business Association 	\$1,200.00
<ul style="list-style-type: none"> ■ Design and install wood, carved Village entry signage as illustrated in the Entryway Sketches in this Plan. Three entry signs at \$3,000.00 each <ul style="list-style-type: none"> • Felton Town Association • Felton Business Association • Community Organizations and Service Clubs • S.L.V. Chamber of Commerce 	\$9,000.00
<ul style="list-style-type: none"> ■ Designation and signing of designated equestrian paths and equestrian crossings. <ul style="list-style-type: none"> • Local Equestrian Groups • County Public Works Department • County Parks • County Planning Department 	\$3,000.00

Major Improvements

Description	Cost Estimate ⁽¹⁾	Projected Construction	Funding Sources
■ Felton Covered Bridge Restoration including Streambank Stabilization	\$ 350,000.00	Completed January 1987	<ul style="list-style-type: none"> • State/Federal Grants • Central Coast Resource Conservation and Development Council • County Parks
■ Utility Undergrounding:		Design/Engineering Started 1986; Construction Programmed for 1988	<ul style="list-style-type: none"> • PG&E Rule 20A Funds.
• Underground Utility District, Felton No. 10, See Utility District Boundary Map on Page 11	\$1,140,000.00		
• North Covered Bridge Road Pacific Bell Telephone Cable Line	\$ 65,000.00	Fall 1987	<ul style="list-style-type: none"> • Pacific Bell as part of system-wide upgrading
■ Graham Hill Road Improvements:		Project Design Started 1986; Construction Programmed for 1988.	<ul style="list-style-type: none"> • Cal Trans • County Road Funds
• Bridge/Roadway/Highway 9 Intersection Improvements	\$1,200,000.00		
• Sidewalks, Raised Landscaped Medians and Mount Hermon Road Signalization	\$ 250,000.00		
■ Highway 9 Improvement Plan from Graham Hill Road south to Hihn Street, as illustrated on pages 29-30— Highway 9 Design Plan , and including: sidewalks, continuous left turn lane of textured pavers, raised landscaped entry islands and crosswalks of textured paving stones.	\$ 450,000.00	Coordinate with Graham Hill Road/ Highway 9 Intersection Improvements. Road improvements	<ul style="list-style-type: none"> • Cal Trans • Business Improvement District • Business and Property Owners District
■ Felton Commons:		To be determined	<ul style="list-style-type: none"> • County Parks • Cal Trans Park & Ride Funding
• Land Purchase	undetermined		
• Improvements Plan as illustrated on Page 20	\$ 275,000.00		
■ Long range plan to develop alternate road access from Henry Cowell State Park to existing privately owned horse stables south of the Felton Covered Bridge	undetermined	To be determined	To be determined

(1) Cost Estimates are based upon 1987 Unit Costs and upon coordination of road improvements with utility undergrounding.